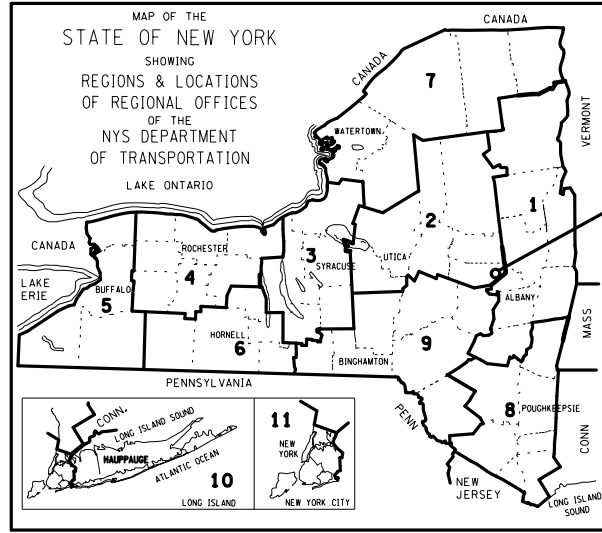


PROJECT MANAGER S. KERN  
 CHECK S. KERN  
 DRAFTING F. CUOCCIO  
 CHECK B. WEAVER  
 DESIGN S. KERN  
 JOB MANAGER D. THOMAS  
 DESIGN SUPERVISOR J. SIMKULET



PROJECT LOCATION



CEMETERY DRIVE OVER THE SOUTH CHUCTANUNDA CREEK BRIDGE REPLACEMENT (BIN 3310200)

TOWN OF FLORIDA  
 MONTGOMERY COUNTY  
 NEW YORK

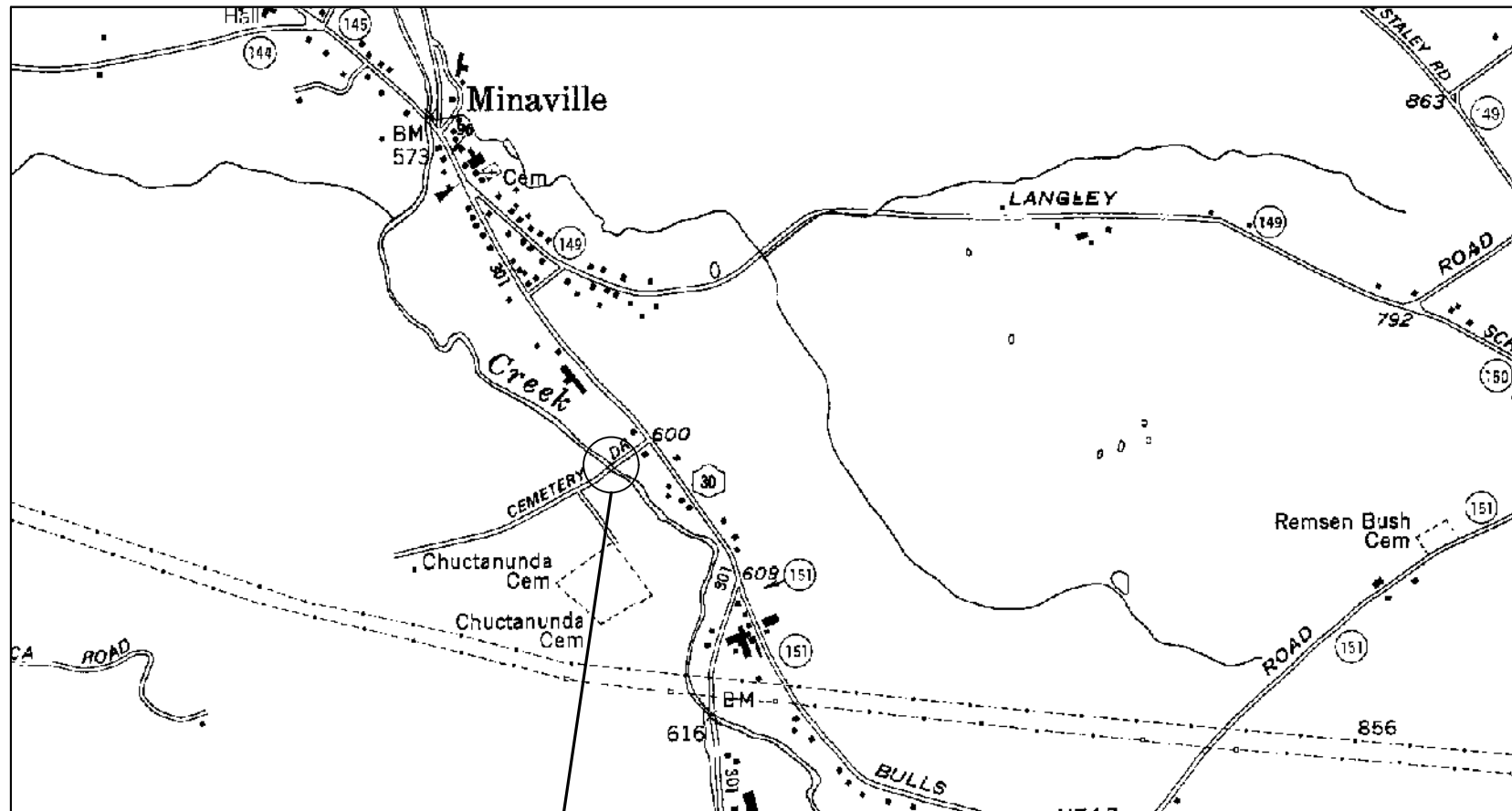
D036212

45 SHEETS

FEDERAL AID CONTRACT NO. 2754.54

MONTGOMERY COUNTY

|                       |       |
|-----------------------|-------|
| CONTRACTOR'S NAME     | _____ |
| AWARD DATE            | _____ |
| COMPLETION DATE       | _____ |
| FINAL ACCEPTANCE DATE | _____ |
| ENGINEER IN CHARGE    | _____ |
| FINAL COST TOTAL      | _____ |



PROJECT LOCATION

PROJECT LOCATION  
 400 FT. FROM THE INTERSECTION OF ROUTE 30 AND  
 CEMETERY DRIVE IN THE TOWN OF FLORIDA,  
 MONTGOMERY COUNTY  
 NOT TO SCALE

THE LATEST REVISIONS OF THE STANDARD SHEETS MAINTAINED BY THE DEPARTMENT, WHICH ARE CURRENT ON THE DATE OF ADVERTISEMENT FOR BIDS, SHALL BE CONSIDERED TO BE IN EFFECT. ALL PAY ITEMS AND WORK CONTAINED IN THE CONTRACT AND ANY ADDITIONAL PAY ITEMS AND WORK ENCOUNTERED DURING THE COURSE OF THE CONTRACT SHALL BE SUBJECT TO THE APPLICABLE STANDARD SHEET(S) UNLESS OTHERWISE SPECIFIED IN THE CONTRACT DOCUMENTS.

ALL WORK CONTEMPLATED UNDER THIS CONTRACT IS TO BE COVERED BY AND IN CONFORMITY WITH THE STANDARD SPECIFICATIONS (US CUSTOMARY) REFERENCED IN THE CONTRACT PROJECT "PROPOSAL" EXCEPT AS MODIFIED BY THESE PLANS OR BY CHANGES SET FORTH IN THE CONTRACT PROJECT "PROPOSAL".

CONTRACT PLANS HAVE BEEN DESIGNED IN ACCORDANCE WITH NYSOT POLICIES AND GUIDELINES.

PREPARED AND RECOMMENDED BY:



SHERI KERN, P.E.  
 PROJECT MANAGER/STRUCTURAL ENGINEER

JOHN SIMKULET, P.E.  
 VICE PRESIDENT/DIRECTOR OF BRIDGE ENGINEERING

**GPI**

GREENMAN-PEDERSEN, INC.  
 CONSULTING ENGINEERS  
 80 WOLF ROAD, SUITE 300  
 ALBANY, NY 12205

APPROVED BY

*Eric Mead*

ERIC MEAD, COMMISSIONER OF PUBLIC WORKS  
 MONTGOMERY COUNTY



DATE

|  |       |           |
|--|-------|-----------|
| CEMETERY DRIVE OVER THE                    |       |           |
| SOUTH CHUCTANUNDA CREEK                    |       |           |
| BRIDGE REPLACEMENT (BIN 3310200)           |       |           |
| TOWN OF FLORIDA                            |       |           |
| COUNTY: MONTGOMERY                         |       |           |
| FED. ROAD REG. NO.                         | STATE | SHEET NO. |
| 2  | N.Y.  | 1         |
| CAPITAL PROJECT IDENTIFICATION NO. 2754.54 |       |           |

INDEX ON SHEET NO. 2

| ALIGNMENT                    |  | TOPOGRAPHY (MISCELLANEOUS) |                                | UTILITIES   |  |
|------------------------------|--|----------------------------|--------------------------------|---|--|
| ABBR.                        | DESCRIPTION                                  | ABBR.                      | DESCRIPTION                    | ABBR.   | DESCRIPTION  |
| AH                           | AHEAD  | ABUT                       | ABUTMENT                       | E   | ELECTRIC   |
| AZ                           | AZIMUTH                                      | AOBE                       | AS ORDERED BY ENGINEER         | EMH   | ELECTRIC MANHOLE   |
| BK                           | BACK   | ASPH                       | ASPHALT                        | G   | GAS  |
| B                            | BASELINE                                     | BDY                        | BOUNDARY                       | GP  | GUY POLE   |
| BRC                          | BEARING                                      | BLDG                       | BUILDING                       | GSB   | GAS SERVICE BOX (HOUSE LINE)   |
| C                            | CENTERLINE                                   | BM                         | BENCH MARK                     | GV  | GAS VALVE (MAIN LINE)  |
| CS                           | CURVE TO SPIRAL                              | CC                         | CENTER TO CENTER               | HYD   | HYDRANT  |
| e                            | SUPERELEVATION RATE (CROSS SLOPE)            | CONC                       | CONCRETE                       | LP  | LIGHT POLE   |
| EQ                           | EQUALITY                                     | CONST                      | CONSTRUCTION                   | LPG   | LOW PRESSURE GAS   |
| EXT                          | EXTERNAL                                     | CR                         | COUNTY ROAD                    | PP  | POWER POLE   |
| HCL                          | HORIZONTAL CONTROL LINE                      | D                          | DEED DISTANCE                  | SA  | SANITARY SEWER   |
| HSD                          | HEADLIGHT SIGHT DISTANCE                     | DM                         | DIRECT MEASUREMENT             | SMH   | SANITARY MANHOLE   |
| L                            | LENGTH OF CIRCULAR CURVE                     | DWY                        | DRIVEWAY                       | ST  | STORM SEWER  |
| LS                           | LENGTH OF SPIRAL                             | EP                         | EDGE OF PAVEMENT               | T   | TELEPHONE  |
| LVC                          | LENGTH OF VERTICAL CURVE                     | ES                         | EDGE OF SHOULDER               | TCB   | TRAFFIC CONTROL BOX  |
| E                            | CENTER CORRECTION OF VERTICAL CURVE          | FEE                        | FEE ACQUISITION                | TELBOX  | TELEPHONE BOX  |
| M                            | MAIN LINE                                    | FEE WO/A                   | FEE ACQUISITION WITHOUT ACCESS | TEL P   | TELEPHONE POLE   |
| PC                           | POINT OF CURVATURE                           | FP                         | FENCE POST                     | TMH   | TELEPHONE MANHOLE  |
| PI                           | POINT OF INTERSECTION                        | FD                         | FOUNDATION                     | CTV   | CABLE TELEVISION   |
| POL                          | POINT ON LINE                                | FL                         | FENCE LINE                     | W   | WATER  |
| PSD                          | PASSING SIGHT DISTANCE                       | GAR                        | GARAGE                         | WSB   | WATER SERVICE BOX (HOUSE LINE)   |
| PT                           | POINT OF TANGENT                             | GR                         | GRAVEL                         | WV  | WATER VALVE (MAIN LINE)  |
| PVC                          | POINT OF VERTICAL CURVE                      | HO                         | HOUSE                          | <b>SUBSURFACE EXPLORATION</b>                                       |  |
| PVI                          | POINT OF VERTICAL INTERSECTION               | HWY                        | HIGHWAY                        |   |  |
| PVT                          | POINT OF VERTICAL TANGENT                    | IP                         | IRON PIN OR IRON PIPE          | ABBR.   | DESCRIPTION  |
| R                            | RADIUS                                       | MB                         | MAILBOX                        | <b>REPLACE ABBREVIATION "AB" WITH:</b>                              |  |
| SC                           | SPIRAL TO CURVE                              | MON                        | MONUMENT                       | AH  | HAND AUGER   |
| SSD                          | STOPPING SIGHT DISTANCE                      | N&W                        | NAIL AND WASHER                | CP  | CONE PENETROMETER  |
| ST                           | SPIRAL TO TANGENT                            | OG                         | ORIGINAL GROUND                | DA  | 2 1/4 INCHES CASED DRILL HOLE  |
| STA                          | STATION                                      | O/H                        | OVERHEAD                       | DM  | DRILLING MUD   |
| T                            | TANGENT LENGTH                               | P                          | PARCEL                         | DN  | 4 INCHES CASED DRILL HOLE  |
| TGL                          | THEORETICAL GRADE LINE                       | PAV'T                      | PAVEMENT                       | FH  | HOLLOW FLIGHT AUGER  |
| TS                           | TANGENT TO SPIRAL                            | PE                         | PERMANENT EASEMENT             | PA  | POWER AUGER  |
| VC                           | VERTICAL CURVE                               | PED POLE                   | PEDESTRIAN POLE                | PH  | PROBE  |
| <b>TOPOGRAPHY (DRAINAGE)</b> |  | P                          | PROPERTY LINE                  | PT  | PERCOLATION TEST HOLE  |
| ABBR.                        | DESCRIPTION                                  | POR                        | PORCH                          | RP  | 1 INCH SAMPLER (RETRACTABLE PLUG)  |
| BB                           | BOTTOM OF BANK (STREAM)                      | RR                         | RAILROAD                       | TO BE DEFINED AT THE TIME OF EXPLORATION                            |  |
| BC                           | BOTTOM OF CURB                               | RTE                        | ROUTE                          | SP  | SEISMIC POINT  |
| BO                           | BOTTOM OF OPENING                            | ROW                        | RIGHT OF WAY                   | TP  | TEST PIT   |
| CAP                          | CORRUGATED ALUMINUM PIPE                     | RW                         | RETAINING WALL                 | <b>ABBREVIATION "C" IN CATAGORIES:<br/>DA, DM, DN, AND FH WITH:</b> |  |
| CB                           | CATCH BASIN                                  | SH                         | STATE HIGHWAY                  | B   | BRIDGE   |
| CIP                          | CAST IRON PIPE                               | SHLDR                      | SHOULDER                       | C   | CUT  |
| C STRM                       | CENTERLINE OF STREAM                         | SPK                        | SPIKE                          | D   | DAM  |
| CMP                          | CORRUGATED METAL PIPE                        | ST                         | STREET                         | F   | FILL   |
| CP                           | CONCRETE PIPE                                | STK                        | STAKE                          | K   | CULVERT  |
| CSP                          | CORRUGATED STEEL PIPE                        | STY                        | STORY                          | W   | WALL   |
| CULV                         | CULVERT                                      | SW                         | SIDEWALK                       | X   | TO BE USED IF ONE OF THE ABOVE CANNOT<br>BE DEFINED AT THE TIME THE EXPLORATION<br>IS MADE |
| DIA                          | DIAMETER                                     | TE                         | TEMPORARY EASEMENT             |   |  |
| DMH                          | DRAINAGE MANHOLE                             | TO                         | TEMPORARY OCCUPANCY            |   |  |
| DS                           | DRAINAGE STRUCTURE PIPE                      | U/G                        | UNDERGROUND                    |   |  |
| D'XING                       | DITCH CROSSING                               | WW                         | WING WALL                      |   |  |
| EHW                          | EXTREME HIGH WATER                           |                            |                                |   |  |
| EL                           | ELEVATION                                    |                            |                                |   |  |
| ELEV                         | ELEVATION                                    |                            |                                |   |  |
| ELW                          | EXTREME LOW WATER                            |                            |                                |   |  |
| ES                           | END SECTION                                  |                            |                                |   |  |
| HW                           | HEADWALL                                     |                            |                                |   |  |
| INV                          | INVERT                                       |                            |                                |   |  |
| MH                           | MANHOLE                                      |                            |                                |   |  |
| MHW                          | MEAN HIGH WATER                              |                            |                                |   |  |
| OHW                          | ORDINARY HIGH WATER                          |                            |                                |   |  |
| OLW                          | ORDINARY LOW WATER                           |                            |                                |   |  |
| RCP                          | REINFORCED CONCRETE PIPE                     |                            |                                |   |  |
| SICPP                        | SMOOTH INTERIOR CORRUGATED POLYETHYLENE PIPE |                            |                                |   |  |
| TB                           | TOP OF BANK (STREAM)                         |                            |                                |   |  |
| TC                           | TOP OF CURB                                  |                            |                                |   |  |
| TG                           | TOP OF GRATE                                 |                            |                                |   |  |
| VCP                          | VITRIFIED CLAY PIPE                          |                            |                                |   |  |

| STANDARD SYMBOL (PLANS) | ITEM PAYMENT UNIT: ESTIMATE OF QUANTITIES SHEET | EQUIVALENT NOMENCLATURE: (SPECS/PROPOSAL) |
|-------------------------|---|---|
| "                       | -   | INCHES                                    |
| '                       | LF  | LINEAR FEET                               |
| mi                      | MI  | MILES                                     |
| ft <sup>2</sup>         | SF  | SQUARE FEET                               |
| YD <sup>2</sup>         | SY  | SQUARE YARD                               |
| AC                      | AC  | ACRES                                     |
| YD <sup>3</sup>         | CY  | CUBIC YARD                                |
| GAL                     | GAL   | GALLON                                    |
| lb                      | LB  | POUND                                     |
| TON                     | TON   | TON                                       |

|  |  |             |                    |          |   |   |   |
|--|--|-------------|--------------------|----------|---|---|---|
| AS-BUILT REVISIONS<br>DESCRIPTION OF ALTERATIONS:  | CEMETERY DRIVE OVER THE                    | PIN 2754.54 | BRIDGES<br>3310200 | CULVERTS | ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED | CONTRACT NUMBER<br>D036212  |   |
|  | SOUTH CHUCTANUNDA CREEK BRIDGE REPLACEMENT |             |                    |          |   |   |   |
|  | TOWN OF FLORIDA                            |             |                    |          |   |   |   |
|  | COUNTY: MONTGOMERY                         | REGION: 2   |                    |          | INDEX AND ABBREVIATIONS                     | DRAWING NO. IND-1<br>SHEET NO. 2  |   |
| IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION. |  |             |                    |          |   |  GREENMAN-PEDERSEN, INC.<br>CONSULTING ENGINEERS |  MONTGOMERY COUNTY<br>DEPARTMENT OF PUBLIC WORKS |

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| ALIGNMENT  |         |                            | LANDSCAPE              |         |                                    | ROADWAY                  |         |                              | UTILITIES |       |                                  |
|--|---------|----------------------------|------------------------|---------|------------------------------------|--------------------------|---------|------------------------------|-----------|-------|----------------------------------|
| STYLE  | NAME    | DESCRIPTION                | STYLE                  | NAME    | DESCRIPTION                        | STYLE                    | NAME    | DESCRIPTION                  | STYLE     | NAME  | DESCRIPTION                      |
|  | AC      | CONTROL (CENTERLINE)       |                        | LABL    | AREA, BRUSH LINE                   |                          | RCZ_P   | CLEAR ZONE                   |           | UC    | CONDUIT, UNDERGROUND             |
|  | AD_P    | DETOUR                     |                        | LAHR    | AREA, HEDGE ROW                    |                          | RG      | GUIDE RAIL, MISCELLANEOUS    |           | UCH   | CONDUIT, HANGING                 |
|  | AT_P    | TRANSITION CONTROL         |                        | LAPB    | AREA, PLANTING BED                 |                          | RGB     | GUIDE RAIL, BOX BEAM         |           | UCO   | CONDUIT, OVERHEAD                |
| <b>BRIDGE</b>  |         |                            |                        | LAWA    | AREA, WOODED AREA OUTLINE          |                          | RGBM    | GUIDE RAIL, BOX BEAM, MEDIAN |           | UE    | ELECTRIC LINE, UNDERGROUND       |
|  | BR      | RAIL                       |                        | LAWE    | AREA, WATERS EDGE                  |                          | RGC     | GUIDE RAIL, CABLE            |           | UEH   | ELECTRIC LINE, HANGING           |
|  | BSHT    | SHEET PILING               |                        | LCUT_P  | CUT LIMIT                          |                          | RGCB    | GUIDE RAIL, CONCRETE BARRIER |           | UEO   | ELECTRIC LINE, OVERHEAD          |
| <b>CONTROL</b>   |         |                            |                        | LFILL_P | FILL LIMIT                         |                          | RGP_P   | GUIDE POST                   |           | UETO  | ELECTRIC TRANSMISSION, OVERHEAD  |
|  | CB      | BASELINE                   |                        | LFNC    | FENCE                              |                          | RGW     | GUIDE RAIL, W BEAM           |           | UESS  | ELECTRIC, SUBSTATIONS            |
|  | CBPR    | BASELINE, PROJECTION       |                        | LTRC    | TREE ROW, CONIFEROUS               |                          | RGWM    | GUIDE RAIL, W BEAM, MEDIAN   |           | UFO   | FIBER OPTIC, UNDERGROUND         |
| <b>DRAINAGE</b>  |         |                            |                        | LTRD    | TREE ROW, DECIDUOUS                |                          | RPB     | PARKING BUMPER               |           | UFOH  | FIBER OPTIC, HANGING             |
|  | DCP     | CULVERT PIPE               |                        | LWHD    | WALL, H PILE                       |                          | RRC     | RAIL ROAD, CATENARY          |           | UFOO  | FIBER OPTIC, OVERHEAD            |
|  | DCP_P   | CULVERT PIPE (DIR)         |                        | LWR     | WALL, RETAINING                    |                          | RRER    | RAIL ROAD, 3RD RAIL          |           | UG    | GAS, UNDERGROUND                 |
|  | DDG_P   | DITCH, GRASS LINED         |                        | LWS     | WALL, STONE                        |                          | RRPLS_P | RAIL, PHOTO, LARGE SCALE     |           | UGH   | GAS, HANGING                     |
|  | DDP_P   | DITCH, PAVED INVERT        | <b>ROW MAPPING</b>     |         |                                    |                          | RRPS    | RAIL, PHOTO, SMALL SCALE     |           | UGO   | GAS, OVERHEAD                    |
|  | DDS_P   | DITCH, STONE LINED         |                        | MDL     | DEED LINE                          |                          | RRPSS   | RAIL, PHOTO, SMALL SCALE     |           | UIC   | INFORM CABLE, UNDERGROUND        |
|  | DFL_P   | FLOW LINE                  |                        | MEE     | EASEMENT, EXISTING                 |                          | RRS     | RUMBLE STRIP                 |           | UICH  | INFORM CABLE, HANGING            |
|  | DSSD    | SLOTTED DRAIN              |                        | MEP_P   | EASEMENT, PERMANENT                |                          | RRSLS_P | RAIL, SURVEY, LARGE SCALE    |           | UO    | OIL LINE, UNDERGROUND            |
|  | DUD_P   | UNDERDRAIN                 |                        | MEPA_P  | EASEMENT, PERMANENT, APPROX.       |                          | RRSSS   | RAIL, SURVEY, SMALL SCALE    |           | UOH   | OIL LINE, HANGING                |
| <b>ENVIRONMENTAL</b>   |         |                            |                        | MET_P   | EASEMENT, TEMPORARY                | <b>SIGNS</b>             |         |                              |           | UPBP  | POLE, BRACE, PUSH BRACE          |
|  | EBLHS   | BALE, STRAW                |                        | META_P  | EASEMENT, TEMPORARY, APPROX.       |                          | SBLB    | BILLBOARDS                   |           | UPGW  | POLE, GUY WIRE                   |
|  | ECT     | CURTAIN, TURBIDITY         |                        | MF_P    | FEE ACQUISITION, W/ ACCESS         |                          | SM      | MULTIPLE POST                |           | USA   | SANITARY SEWER, UNDERGROUND      |
|  | EDMC    | DAM, COFFER TYPE           |                        | MFA_P   | FEE ACQUISITION, APPROXIMATE       |                          | SSO     | STRUCTURE, OVERHEAD          |           | USAH  | SANITARY SEWER, HANGING          |
|  | EDMEC_P | DAM, EARTHEN, CHECK        |                        | MFS_P   | FEE ACQUISITION, SHAPE             |                          | SSOC    | STRUCTURE, OVHD. CANTILEVER  |           | USAF  | SANITARY SEWER, FORCE MAIN, UGND |
|  | EDMPC_P | DAM, PREFAB, CHECK         |                        | MFWOA_P | FEE ACQUISITION, W/O ACCESS        | <b>STRIPING</b>          |         |                              |           | USAFH | SANITARY SEWER, FORCE MAIN, HANG |
|  | EDMSC_P | DAM, STONE, CHECK          |                        | MHA     | HISTORICAL, ACQUISITION            |                          | STB*    | BROKEN LINE                  |           | UT    | TELEPHONE, UNDERGROUND           |
|  | EFNS    | FENCE, SILT                |                        | MHB     | HIGHWAY BOUNDARY                   |                          | STDB*   | DOUBLE BROKEN LINE           |           | UTH   | TELEPHONE, HANGING               |
|  | EFNSV   | FENCE, SILT & VEGETATION   |                        | MHBA    | HIGHWAY BOUNDARY, APPROX.          |                          | STD*    | DOTTED LINE LONG             |           | UTO   | TELEPHONE, OVERHEAD              |
|  | EFNV    | FENCE, VEGETATION          |                        | MHBW    | HWY BOUNDARY, FACE OF WALL         |                          | STDS*   | DOTTED LINE SHORT            |           | UTV   | CABLE TV, UNDERGROUND            |
|  | EWAA_P  | WETLAND, ADJACENT AREA     |                        | MHBWOA  | HIGHWAY BOUNDARY, W/O ACCESS       |                          | STFB*   | FULL BARRIER LINE            |           | UTVH  | CABLE TV, HANGING                |
|  | EFW     | WETLAND, FEDERAL           |                        | MJC     | JURISDICTION, CITY                 |                          | STH*    | HATCH LINE                   |           | UTVO  | CABLE TV, OVERHEAD               |
|  | EFWS    | WETLAND, FEDERAL AND STATE |                        | MJCY    | JURISDICTION, COUNTY               |                          | STPB*   | PARTIAL BARRIER LINE         |           | UUU   | UNKNOWN, UNDERGROUND             |
|  | EFWM    | WETLAND, MITIGATION AREA   |                        | MJHD    | JURISDICTION, HISTORIC DISTRICT    |                          | STRCT   | ROUNDAABOUT, CAT TRACKS      |           | UUH   | UNKNOWN, HANGING                 |
|  | EFWS    | WETLAND, MITIGATION AREA   |                        | MJLL    | JURIS., (GREAT, MILITARY) LOT LINE |                          | STRYL   | ROUNDAABOUT, YIELD LINE      |           | UUO   | UNKNOWN, OVERHEAD                |
|  | EFWS    | WETLAND, MITIGATION AREA   |                        | MJN     | JURISDICTION, NATION               |                          | STSB    | STOP BAR                     |           | UW    | WATER LINE, UNDERGROUND          |
|  | EFWS    | WETLAND, MITIGATION AREA   |                        | MJPB    | JURISDICTION, PUBLIC LANDS         |                          | STSE*   | SOLID, EDGE                  |           | UWH   | WATER LINE, HANGING              |
|  | EFWS    | WETLAND, MITIGATION AREA   |                        | MJS     | JURISDICTION, STATE                |                          | STXL*   | X WALK, LADDER LINE          |           | UWO   | WATER LINE, OVERHEAD             |
|  | EFWS    | WETLAND, MITIGATION AREA   |                        | MJT     | JURISDICTION, TOWN                 |                          |         |                              |           |       |                                  |
|  | EFWS    | WETLAND, MITIGATION AREA   |                        | MJV     | JURISDICTION, VILLAGE              |                          |         |                              |           |       |                                  |
|  | EFWS    | WETLAND, MITIGATION AREA   |                        | MPL     | PROPERTY LOT LINE                  |                          |         |                              |           |       |                                  |
|  | EFWS    | WETLAND, MITIGATION AREA   |                        | MPLA    | PROPERTY LOT LINE, APPROXIMATE     |                          |         |                              |           |       |                                  |
|  | EFWS    | WETLAND, MITIGATION AREA   |                        | MSL     | SUB LOT LINE                       |                          |         |                              |           |       |                                  |
| <b>NOTES:</b>  |         |                            | <b>TRAFFIC CONTROL</b> |         |                                    | <b>TRAFFIC WORK ZONE</b> |         |                              |           |       |                                  |
| 1. THE LEGEND ILLUSTRATES MAPPING FEATURES (EXISTING AND PROPOSED).  |         |                            |                        |         |                                    |                          |         |                              |           |       |                                  |
| 2. FEATURES ARE SHOWN AS EITHER LINEAR (ROADWAY GUIDERAIL, ROADWAY SIDEWALK, UTILITY LINES, ETC.) OR POINT (SIGN, UTILITY POLE, ETC.).   |         |                            |                        |         |                                    |                          |         |                              |           |       |                                  |
| 3. FEATURES SHOWN ON THE LEGEND AS EXISTING FEATURES ALSO HAVE CORRESPONDING PROPOSED FEATURES.  |         |                            |                        |         |                                    |                          |         |                              |           |       |                                  |
| 4. PROPOSED FEATURE SYMBOLOGY IS IDENTICAL TO EXISTING FEATURE SYMBOLOGY EXCLUDING LINE WEIGHT. LINE WEIGHT FOR PROPOSED FEATURES IS THICKER (0.015 in ON B SIZE DRAWINGS).      |         |                            |                        |         |                                    |                          |         |                              |           |       |                                  |
| 5. MAPPING FEATURES NOT INCLUDED ON THE LEGEND SHEET DO NOT HAVE A UNIQUE SYMBOLOGY (SUCH AS THE PAVEMENT EDGE, PAVEMENT EDGE OF TRAVEL WAY) AND SHOULD BE LABELED ON THE PLANS. |         |                            |                        |         |                                    |                          |         |                              |           |       |                                  |
| 6. FEATURES SHOWN AT THE HEAVIER WEIGHT ARE PROPOSED ONLY AND DO NOT HAVE CORRESPONDING EXISTING FEATURES.   |         |                            |                        |         |                                    |                          |         |                              |           |       |                                  |

|  |   |             |                    |   |   |                                  |
|--|---|-------------|--------------------|---|---|----------------------------------|
| AS-BUILT REVISIONS<br>DESCRIPTION OF ALTERATIONS:  | CEMETERY DRIVE OVER THE<br>SOUTH CHUCTANUNDA CREEK BRIDGE REPLACEMENT | PIN 2754.54 | BRIDGES<br>3310200 | CULVERTS  | ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED | CONTRACT NUMBER<br>D036212       |
|  | TOWN OF FLORIDA   |             |                    |   | LEGEND<br>(1 OF 2)                          | DRAWING NO. LEG-1<br>SHEET NO. 3 |
|  | COUNTY: MONTGOMERY REGION: 2  |             |                    |   |   |                                  |
| IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION. |   |             |                    |   |   |                                  |
| <b>GPI</b> GREENMAN-PEDERSEN, INC.<br>CONSULTING ENGINEERS   |   |             |                    | MONTGOMERY COUNTY<br>DEPARTMENT OF PUBLIC WORKS |   |                                  |

| ALIGNMENT      |         |                              | DRAINAGE             |            |   | ITS  |          |                                | ROW MAPPING            |          |                            | SIGNS                    |         |                           | UTILITIES |       |                         |
|----------------|---------|------------------------------|----------------------|------------|---|------|----------|--------------------------------|------------------------|----------|----------------------------|--------------------------|---------|---------------------------|-----------|-------|-------------------------|
| CELL           | NAME    | DESCRIPTION                  | CELL                 | NAME       | DESCRIPTION   | CELL | NAME     | DESCRIPTION                    | CELL                   | NAME     | DESCRIPTION                | CELL                     | NAME    | DESCRIPTION               | CELL      | NAME  | DESCRIPTION             |
| ⊗              | ACC     | CENTER OF CURVATURE          | +                    | DINV       | INVERT  | ⊕    | IANT_P   | ANTENNAS                       | ⊕                      | MDL1P    | DEED LINE, TYPE 1          | ⊕                        | S       | SINGLE POST               | ⊕         | UEB   | ELECTRIC, BOX           |
| +              | ACOGO   | COGO                         | ▭                    | DS         | STRUCTURE, RECTANGULAR                                      | ⊕    | IASCTS   | ACCOU. SPEED/COUNT SNSR.S      | ⊕                      | MDL2P    | DEED LINE, TYPE 2          | ⊕                        | S.P     | SINGLE POST, PROPOSED     | ⊕         | UEM   | ELECTRIC, METER         |
| ⊙              | ACS     | CURVE TO SPIRAL              | +                    | DSI        | STRUCTURE, INVERT   | ⊕    | ICABPAD  | CABINET & PAD                  | ⊕                      | MDL3P    | DEED LINE, TYPE 3          | ⊕                        | SB.P    | BACK TO BACK, PROPOSED    | ⊕         | UEMH  | ELECTRIC, MANHOLE       |
| △              | ADPL_P  | DETOUR, POINT OF INTERSECT.  | ⊕                    | DSM        | STRUCTURE, MANHOLE  | ⊕    | ICCTV    | CCTV SITE                      | ⊕                      | MDL4P    | DEED LINE, TYPE 4          | ⊕                        | SDEL    | DELINEATORS               | ⊕         | UEPT  | ELECTRIC, POLE, TRANS.  |
| ○              | ADPL_P  | DETOUR, POINT ON LINE        | ⊕                    | DSMTXX_P   | STRUCTURE, MANHOLE, TYPE "XX" "XX" = 48, 60, 72, 96         | ⊕    | ICDPD    | CDPD TRANSCEIVER               | ⊕                      | MDL5P    | DEED LINE, TYPE 5          | ⊕                        | SPM     | PARKING METER             | ⊕         | UGM   | GAS, METER              |
| ○              | AEQN    | EQUATION                     | ⊕                    | DSR        | STRUCTURE, ROUND  | ⊕    | ICELLT   | CELL PHONE TOWER               | ⊕                      | MEEP     | EASEMENT, EXISTING         | ⊕                        | SRM     | REFERENCE MARKERS         | ⊕         | UGMH  | GAS, MANHOLE            |
| ⊕              | AEQNAHD | EQUATION AHEAD               | ⊕                    | DST"X"CB_P | STRUCTURE, RECT., WITH CURB TYPE "X" "X" = F, G, N, O, P, R | ⊕    | ICJTB    | CONDUIT JACK OR BORING         | ⊕                      | MEPAP_P  | EASEMENT, PERM., APPROX.   | ⊕                        | SRSC3   | SHLD, CTY, 123 DIG.       | ⊕         | UGLM  | GAS, LINE MARKER        |
| ⊕              | AEQNBK  | EQUATION BACK                | ⊕                    | DST"X"CB_P | STRUCTURE, RECT., WITH CURB TYPE "X" "X" = F, G, N, O, P, R | ⊕    | ICNTLCAB | CONTROLLER CABINET             | ⊕                      | MEPP_P   | EASEMENT, PERM., BACK LINE | ⊕                        | SRSC4   | SHLD, CTY, 4 DIG.         | ⊕         | UGP   | GAS/FUEL PUMP           |
| ○              | AEVT    | EVENT STATION                | ⊕                    | DST"X"CB_P | STRUCTURE, RECT., WITH CURB TYPE "X" "X" = F, G, N, O, P, R | ⊕    | ICPB     | COMMUNICATION PULL BOX         | ⊕                      | MEPSP_P  | EASEMENT, PERM., SHAPE     | ⊕                        | SRSCT2  | SHLD, CTY TOUR, 1-2 DIG.  | ⊕         | UGV   | GAS, VALVE              |
| ⊙              | APC     | POINT OF CURVATURE           | ⊕                    | DST"X"CB_P | STRUCTURE, RECT., WITH CURB TYPE "X" "X" = F, G, N, O, P, R | ⊕    | ICTD     | CONDUIT TURNING DOWN           | ⊕                      | MFAP_P   | FEE ACQUISITION, APPROX.   | ⊕                        | SRSCT4  | SHLD, CTY TOUR, 3-4 DIG.  | ⊕         | UGVT  | GAS, VENT               |
| ○              | APCC    | POINT OF COMPOUND CURVATURE  | ⊕                    | DST"X"CB_P | STRUCTURE, RECT., WITH CURB TYPE "X" "X" = F, G, N, O, P, R | ⊕    | ICTU     | CONDUIT TURNING UP             | ⊕                      | MFP_P    | FEE ACQUISITION, BACK LINE | ⊕                        | SRSI    | SHLD, INTERSTATE          | ⊕         | ULP   | LIGHTING, POLE          |
| △              | API     | POINT OF INTERSECTION        | <b>ENVIRONMENTAL</b> |            |   | ⊕    | ICVTRT   | COMM. VEH. ROAD TRANSCEIVER    | ⊕                      | MFSP_P   | FEE ACQUISITION, SHAPE     | ⊕                        | SRSN2   | SHLD, NATIONAL, 2 DIG.    | ⊕         | ULPM  | LIGHTING, POLE, MEDIAN  |
| △              | APOB    | POINT OF BEGINNING           |                      |            |   | +    | IDEFAULT | DEFAULT                        | ⊕                      | MHBAP    | HIGHWAY BNDRY., APPROX.    | ⊕                        | MHBCP   | HISTORICAL, BLDG. CORNERS | ⊕         | SRSN3 | SHLD, NATIONAL, 3 DIG.  |
| ○              | APOC    | POINT OF CURVATURE           | ⊕                    | EIOP_P     | STR., INLET, OUTLET PROT.                                   | ⊕    | IEZR     | E-ZPASS READER                 | ⊕                      | MHBP     | HIGHWAY BNDRY, PT.         | ⊕                        | SRSS2   | SHLD, STATE, 2 DIG.       | ⊕         | UMFC  | MISC. FILLER CAP        |
| △              | APOE    | POINT OF END                 | ⊕                    | EIPGB_P    | STR., INLET PROT., GRAVEL BAG                               | ⊕    | IEZTR    | TRANSMITTAL READER             | ⊕                      | MJCP     | PT., JURIS. CITY           | ⊕                        | SRSS3   | SHLD, STATE, 3 DIG.       | ⊕         | UOLM  | OIL, LINE MARKER        |
| ○              | APOL    | POINT ON LINE                | ⊕                    | EIPHS_P    | STR., INLET PROT., HAY/STRAW                                | ⊕    | IFOXCAB  | FIBER OPTIC X-CONNECT CABINET  | ⊕                      | MPBC     | PT., BUILDING CORNER       | <b>TRAFFIC CONTROL</b>   |         |                           | ⊕         | UP    | POLE, WITH UTILITY      |
| ○              | APOS    | POINT ON SPIRAL              | ⊕                    | EIPHS_P    | STR., INLET PROT., HAY/STRAW                                | ⊕    | IFUSSPL  | FUSION SPLICE                  | ⊕                      | MPCC     | PT., CROSS CUT             |                          |         |                           | ⊕         | UPD   | POLE, DEAD (NO UTILITY) |
| ○              | APOT    | POINT ON TANGENT             | ⊕                    | EIPP_P     | STR., INLET PROT., PREFAB.                                  | ⊕    | IHARADV  | HAR ADVISORY SIGN              | ⊕                      | MPDH     | PT., DRILL HOLE            | ⊕                        | TCBJ    | BOX, JUNCTION             | ⊕         | UPL   | POLE, WITH LIGHT        |
| △              | APOVC   | POINT ON VERTICAL CURVE      | ⊕                    | EIPSF_P    | STR., INLET PROT., SILT FENCE                               | ⊕    | IHARST   | HAR SITE                       | ⊕                      | MPF      | PT., FENCE LOCATION        | ⊕                        | TCBP    | BOX, PULL BOX             | ⊕         | USMH  | SANITARY SEWER MANHOLE  |
| △              | APOVT   | POINT ON VERTICAL TANGENT    | ⊕                    | ERCB       | RISER, CONCRETE BOX   | ⊕    | ILC      | LOAD CENTER                    | ⊕                      | MPIR     | PT., IRON PIPE             | ⊕                        | TCBS    | BOX, SPLICE               | ⊕         | UTB   | TELEPHONE, BOOTH        |
| Y              | APORC   | POINT ON REVERSE CURVE       | ⊕                    | ETRS_P     | TRAP, SEDIMENT  | ⊕    | IMECSPL  | MECHANICAL SPLICE              | ⊕                      | MPM      | PT., IRON ROD              | ⊕                        | TCMC    | MICROCOMPUTER CABINET     | ⊕         | UTLM  | TELEPHONE, LINE MARKER  |
| ⊙              | APT     | POINT OF TANGENCY            | +                    | EWFG       | WETLAND FLAG  | ⊕    | IMSCS    | PORT. SPEED & COUNT SENSOR     | ⊕                      | MPMM     | PT., MONUMENT, MISC.       | ⊕                        | TCCP    | PED POLE                  | ⊕         | UTMH  | TELEPHONE, MANHOLE      |
| ⊙              | APVC    | POINT OF VERTICAL CURVATURE  | <b>GEOTECHNICAL</b>  |            |   | ⊕    | IMSCTS   | MICRO SPEED & COUNT SENSOR     | ⊕                      | MPN      | PT., NAIL                  | <b>TRAFFIC WORK ZONE</b> |         |                           | ⊕         | UTVLM | CABLE TV, LINE MARKER   |
| △              | APVCC   | POINT OF VERT. CMPND CURVE   |                      |            |   | ⊕    | IOVHVS   | PERM. OVERHEAD VMS             | ⊕                      | IMT      | MICROWAVE TRANSCEIVER      |                          |         |                           | ⊕         | MPRS  | PT., RAILROAD SPIKE     |
| ⊕              | APVI    | POINT OF VERT. INTERSECTION  | ⊕                    | GDH        | DRILL HOLE  | ⊕    | IPASCS   | PORT. ACCOU. SPD & CNT. SENSOR | ⊕                      | MPSP     | PT., SPIKE                 | ⊕                        | UUB     | UNKNOWN, BOX              | ⊕         | UUB   | UNKNOWN, BOX            |
| △              | APVRC   | POINT OF VERT. REVERSE CURVE | <b>LANDSCAPE</b>     |            |   | ⊕    | IPEDS    | PEDESTRIAN SIGNAL HEAD         | ⊕                      | MPST     | PT., STAKE                 | ⊕                        | UUB     | UNKNOWN, BOX              | ⊕         | UUB   | UNKNOWN, BOX            |
| ⊙              | APVT    | POINT OF VERTICAL TANGENCY   |                      |            |   | +    | LELS     | ELEVATION, SPOT                | ⊕                      | IPSS     | PAVEMENT SURFACE SENSOR    | ⊕                        | MPTW    | PT., TREE W/ WIRE         | ⊕         | UUB   | UNKNOWN, BOX            |
| ⊙              | ASC     | SPIRAL TO CURVE              | ⊕                    | LFP        | FLAG POLE   | ⊕    | IPVMS    | PERM. VMS                      | ⊕                      | MPWL     | PT., WALL LOCATION         | ⊕                        | UUB     | UNKNOWN, BOX              | ⊕         | UUB   | UNKNOWN, BOX            |
| △              | ASPI    | SPIRAL POINT OF INTERSECTION | ⊕                    | LMB        | MAILBOX   | ⊕    | IRM      | RAMP METER                     | <b>ROW ACQUISITION</b> |          |                            | ⊕                        | UUB     | UNKNOWN, BOX              | ⊕         | UUB   | UNKNOWN, BOX            |
| ○              | ASTS    | SPIRAL TO SPIRAL             | ⊕                    | LPB        | PAPER BOX   | ⊕    | IRWIS    | RDWY WEATHER INFO. SENSOR      |                        |          |                            | ⊕                        | MFS_P.T | FEE ACQUISITION           | ⊕         | UUB   | UNKNOWN, BOX            |
| ⊗              | AST     | SPIRAL TO TANGENT            | ⊕                    | LPST       | POST, SINGLE  | ⊕    | ISP      | SOLAR PANEL                    | ⊕                      | MEPS_P.T | EASEMENT, PERMANENT        | ⊕                        | UUB     | UNKNOWN, BOX              | ⊕         | UUB   | UNKNOWN, BOX            |
| ⊗              | ATS     | TANGENT TO SPIRAL            | ⊕                    | LRB        | ROCK, BOULDER   | ⊕    | ISST     | SPREAD SPECT. TRANSCEIVER      | ⊕                      | METS_P.T | EASEMENT, TEMPORARY        | ⊕                        | UUB     | UNKNOWN, BOX              | ⊕         | UUB   | UNKNOWN, BOX            |
| △              | AVEVT   | VERTICAL EVENT POINT         | ⊕                    | LSHC       | SHRUB, CONIFEROUS   | ⊕    | ITDB     | TELEPHONE DEMARCATION BLK      | ⊕                      | METS_P.T | OCCUPANCY, TEMPORARY       | ⊕                        | UUB     | UNKNOWN, BOX              | ⊕         | UUB   | UNKNOWN, BOX            |
| ○              | AVHIGH  | VERTICAL HIGH POINT          | ⊕                    | LSHD       | SHRUB, DECIDUOUS  | ⊕    | ITP      | SUBSURFACE TEMP. PROBE         | ⊕                      | MFS_P.T  | FEE ACQUISITION W/O ACCESS | ⊕                        | UUB     | UNKNOWN, BOX              | ⊕         | UUB   | UNKNOWN, BOX            |
| ○              | AVLOW   | VERTICAL LOW POINT           | ⊕                    | LTD        | TREE, DECIDUOUS   | ⊕    | IVTRT    | VEHICLE TO RDWY TRANSCEIVER    | ⊕                      | RES_P    | ELEVATION, SPOT            | ⊕                        | UUB     | UNKNOWN, BOX              | ⊕         | UUB   | UNKNOWN, BOX            |
| <b>BRIDGE</b>  |         |                              | ⊕                    | LTS        | TREE, STUMP   | ⊕    | IWIMD    | WEIGHT IN MOTION DETECTOR      | <b>ROADWAY</b>         |          |                            | ⊕                        | UUB     | UNKNOWN, BOX              | ⊕         | UUB   | UNKNOWN, BOX            |
| ⊕              | BSC     | BRIDGE, SCUPPER              | ⊕                    | LW_P       | TREE, WELL OR WALL  | ⊕    | IWVR     | WIRELESS VIDEO REPEATER        |                        |          |                            | ⊕                        | RGA     | GUIDE RAIL, ANCHOR        | ⊕         | UUB   | UNKNOWN, BOX            |
| <b>CONTROL</b> |         |                              | +                    | LUKP       | UNKNOWN POINT   | ⊕    | IWVRC    | WIRELESS VIDEO RECEIVER        | ⊕                      | RGP      | GUIDE POST, SINGLE         | ⊕                        | UUB     | UNKNOWN, BOX              | ⊕         | UUB   | UNKNOWN, BOX            |
| △              | CBP     | BASELINE, POINT              | SEE LEG-1 FOR NOTES  |            |   |      |          |                                |                        |          |                            |                          |         |                           |           |       |                         |
| ○              | CBPOL   | BASELINE, POINT ON LINE      |                      |            |   |      |          |                                |                        |          |                            |                          |         |                           |           |       |                         |
| ⊕              | CBSP    | BASELINE, SPUR POINT         |                      |            |   |      |          |                                |                        |          |                            |                          |         |                           |           |       |                         |
| ⊗              | CBTP    | BASELINE, TIE POINT          |                      |            |   |      |          |                                |                        |          |                            |                          |         |                           |           |       |                         |
| ⊕              | CPBM    | BENCHMARK                    |                      |            |   |      |          |                                |                        |          |                            |                          |         |                           |           |       |                         |
| ⊕              | CPH     | POINT, HORIZ. PHOTOGRAMMETRY |                      |            |   |      |          |                                |                        |          |                            |                          |         |                           |           |       |                         |
| ⊕              | CPSM    | POINT, SURVEY MARKER, PERM.  |                      |            |   |      |          |                                |                        |          |                            |                          |         |                           |           |       |                         |
| ⊕              | CPSV    | POINT, VERT., PHOTOGRAMMETRY |                      |            |   |      |          |                                |                        |          |                            |                          |         |                           |           |       |                         |

|  |   |             |                    |          |  |   |
|--|---|-------------|--------------------|----------|--|---|
| AS-BUILT REVISIONS<br>DESCRIPTION OF ALTERATIONS:  | CEMETERY DRIVE OVER THE<br>SOUTH CHUCTANUNDA CREEK BRIDGE REPLACEMENT | PIN 2754.54 | BRIDGES<br>3310200 | CULVERTS | ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED                | CONTRACT NUMBER<br>D036212                      |
|  | TOWN OF FLORIDA   |             |                    |          | LEGEND<br>(2 OF 2)   | DRAWING NO. LEG-2                               |
|  | COUNTY: MONTGOMERY  | REGION: 2   |                    |          |  | SHEET NO. 4                                     |
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**WORK ZONE TRAFFIC CONTROL NOTES:**

1. THE FOLLOWING NOTES ARE INTENDED TO SUPPLEMENT AND CLARIFY THE REQUIREMENTS SET FORTH IN SECTION 619 OF THE NYS STANDARD SPECIFICATIONS AND SECTION 619 OF THE NYS STANDARD SHEETS.
2. ALL WORK ZONE TRAFFIC CONTROL ACTIVITIES SHALL BE PERFORMED IN ACCORDANCE WITH THE NYS DOT STANDARD SPECIFICATIONS, THE NATIONAL MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) AND NYS SUPPLEMENT.
3. TRAFFIC SHALL BE MAINTAINED IN ACCORDANCE WITH ALL PROVISIONS OF ITEM 619.01 - BASIC WORK ZONE TRAFFIC CONTROL, OR AS AMENDED ON THESE PLANS, FOR THE DURATION OF THE PROJECT.
4. THE CONTRACTOR MAY SUBMIT REVISIONS TO THESE PLANS, IN WRITING, TO THE ENGINEER FOR APPROVAL, HOWEVER ANY COSTS RESULTING FROM THESE CHANGES SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
5. ALL VEHICLES AND EQUIPMENT THAT SHALL BE MOVING IN AND OUT OF TRAFFIC AT WORK AREAS SHALL BE EQUIPPED WITH AN APPROVED AMBER ROTATING SAFETY LIGHT. THIS LIGHT SHALL BE MOUNTED SO AS TO BE EASILY SEEN BY APPROACHING TRAFFIC.
6. VEHICLES BELONGING TO THE CONTRACTOR, OR THE CONTRACTOR'S EMPLOYEES, SHALL NOT BE PARKED ON THE PAVEMENT OR SHOULDERS, OR WITHIN 30 FEET OF THE EDGE OF PAVEMENT ALONG OR ADJACENT TO OPEN TRAVEL LANES.
7. THE CONTRACTOR SHALL NOT PARK EQUIPMENT, NOR STORE MATERIAL, OVERNIGHT WHERE IT IS DEEMED BY THE ENGINEER TO BE A SAFETY HAZARD TO TRAFFIC.
8. DRIVING AGAINST TRAFFIC AT ANY TIME, REGARDLESS OF WHETHER OR NOT THE AREA HAS BEEN CLOSED TO TRAFFIC, SHALL NOT BE PERMITTED, EXCEPT FOR TRAFFIC CONE PICK-UP OR AS SPECIFICALLY PERMITTED BY THE ENGINEER.
9. ESCORT VEHICLES EQUIPPED WITH AN AMBER LIGHT OR AN OPERATING ARROW PANEL WILL BE REQUIRED WHEN TRANSPORTING SLOW MOVING CONSTRUCTION EQUIPMENT ALONG ANY PORTION OF THE ROADWAY OPEN TO TRAFFIC.
10. WHEN REOPENING DRIVING LANES TO TRAFFIC, THE CONTRACTOR SHALL START BY MOVING THE DEVICES AT THE FAR END OF THE LANE CLOSURE AND WORKING TOWARDS THE SIGNS AT THE BEGINNING OF THE LANE CLOSURE. THE SIGNS ARE NOT TO BE TAKEN DOWN UNTIL ALL TRAFFIC CONTROL DEVICES HAVE BEEN REMOVED.
11. SEE TABLE 619-3 IN THE NYS DOT STANDARD SPECIFICATIONS FOR REQUIRED TREATMENT OF PAVEMENT EDGE DROP-OFFS AND DELINEATION.
12. EXCAVATIONS THAT PRODUCE DROP-OFFS ON BOTH SIDES OF THE TRAVELED WAY AT THE SAME TIME SHALL NOT BE PERMITTED. SHOULDER AREAS SHALL BE PREPARED TO RECEIVE THE SHOULDER PAVEMENT MATERIAL IMMEDIATELY AHEAD OF SHOULDER PAVING OPERATIONS TO MINIMIZE THE TIME A DROP-OFF CONDITION EXISTS. "NO SHOULDER" (NYW4-13) SIGNS SHALL BE ERECTED A MINIMUM OF 500 FEET APART THROUGHOUT THE PROJECT WHERE EVER A DROP-OFF EXISTS. LOW SHOULDER SIGNS (W8-9) MAY ALSO BE REQUIRED. IN NO CASE SHALL AN EDGE DROP-OFF EXCEED 3 INCHES.
13. COSTS FOR ALL TEMPORARY SIGNS FOR WORK ZONE TRAFFIC CONTROL SHALL BE INCLUDED UNDER ITEM 619.01.
14. THE MOUNTING OF ALL TEMPORARY CONSTRUCTION SIGNS SHALL BE PER FIGURE 6F-1 CONVENTIONAL ROAD IN THE MUTCD.
15. THE COST OF PROVIDING AND MAINTAINING SAFE AND ADEQUATE INGRESS AND EGRESS TO AND FROM INTERSECTING HIGHWAYS, HOMES AND COMMERCIAL ESTABLISHMENTS AT ALL TIMES, TO THE SATISFACTION OF THE ENGINEER, SHALL BE BORNE BY THE CONTRACTOR, INCLUDING PROVIDING TEMPORARY ASPHALT PAVEMENT TO MAINTAIN THIS ACCESS.
16. IF THE ENGINEER NOTIFIES THE CONTRACTOR OR HIS SUPERINTENDENT OF ANY HAZARDOUS CONSTRUCTION PRACTICES, ALL OPERATIONS IN THAT AREA SHALL BE DISCONTINUED AND IMMEDIATE REMEDIAL ACTION SHALL BE TAKEN TO THE SATISFACTION OF THE ENGINEER BEFORE WORK IS RESUMED.
17. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAKING SURE THAT ALL SIGNS, CONES, FLASHERS, BARRIERS, ETC. ARE IN PLACE AND IN GOOD CONDITION. THE SOLE JUDGE OF THE EFFECTIVENESS OF THE CONTRACTOR'S EFFORTS TOWARDS THE PROTECTION OF TRAFFIC AND PERSONNEL SHALL BE THE ENGINEER.
18. FLAGGERS SHALL BE LOCATED AT ALL ACTIVE WORK AREAS AND AT OTHER LOCATIONS WITHIN A WORK AREA WHERE RESTRICTED SIGHT DISTANCE IMPEDES THE FLOW OF TRAFFIC OR A.O.B.E.

**WORK ZONE TRAFFIC CONTROL NOTES:**

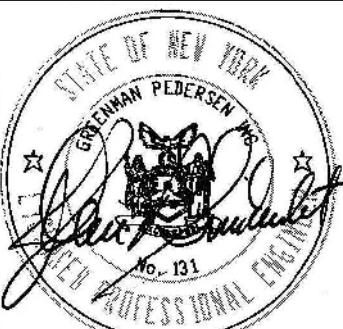
19. EXISTING TRAFFIC SIGNS SHALL BE COVERED AND UNCOVERED AS NECESSARY DURING CONSTRUCTION. COST TO BE INCLUDED UNDER ITEM 619.01.
20. IF IN THE ENGINEER'S JUDGMENT, FLAGS ON SIGNS ARE NECESSARY DUE TO LIMITED SIGHT DISTANCE. THEY SHALL BE PROVIDED BY THE CONTRACTOR. COST SHALL BE INCLUDED IN ITEM 619.01.
21. THERE SHALL BE NO NIGHT WORK BETWEEN THE HOURS OF 7:00 PM AND 7:00 AM EXCEPT AS REQUIRED FOR UTILITY SERVICE INTERRUPTIONS AND AS APPROVED BY THE ENGINEER.
22. DELINEATION WITH REFLECTORIZED PLASTIC DRUMS SHALL BE USED ALONG EMBANKMENTS, AND AT OTHER LOCATIONS WHERE EXISTING GUIDE RAIL HAS BEEN REMOVED, AND SHALL REMAIN IN PLACE UNTIL SATISFACTORY PROTECTION HAS BEEN PROVIDED. SPACING OF DRUMS SHALL BE CONSISTENT WITH THE CONTRACT DOCUMENTS AND AS DIRECTED BY THE ENGINEER.
23. THE CONTRACTOR SHALL MAINTAIN STABLE EXCAVATION SIDE SLOPES AT ALL TIMES.
24. IT IS THE CONTRACTOR'S RESPONSIBILITY TO COORDINATE WITH ALL PUBLIC AND PRIVATE UTILITIES FOR MAINTENANCE OR RELOCATION WORK WITH RESPECT TO SITE ACCESS, TRAFFIC CONTROL AND SCHEDULING TO AVOID CONFLICTS FOR TIMELY COMPLETION OF THE WORK.
25. THE CONTRACTOR SHALL PROVIDE SAFE AND CONVENIENT EMERGENCY ACCESS FOR LOCAL FIRE, POLICE AUTHORITIES AND AMBULANCE SERVICES THROUGHOUT THE PROJECT AREA AT ALL TIMES.
26. RESPONSIBILITY FOR EMERGENCY REPAIRS: THE CONTRACTOR SHALL, IN WRITING, SUBMIT TO THE APPROPRIATE LAW ENFORCEMENT AND GOVERNMENT AGENCIES THE NAME, ADDRESS AND TELEPHONE NUMBER(S) OF THE PERSON OR PERSONS AUTHORIZED TO SECURE LABOR, MATERIALS AND EQUIPMENT FOR EMERGENCY REPAIRS OUTSIDE OF NORMAL WORKING HOURS. DUPLICATE COPIES OF THE ABOVE SHALL BE FILED WITH THE ENGINEER.
27. WHERE DRUMS, CONES, VERTICAL PANELS OR TUBULAR MARKERS ARE USED IN CONTROLLING THE MOVEMENT OF TRAFFIC, THE CONTRACTOR SHALL TAKE WHATEVER STEPS ARE NECESSARY TO PREVENT ALL TRAFFIC CONTROL DEVICES FROM BEING BLOWN OVER OR DISPLACED BY PASSING VEHICLES. THE CONTRACTOR SHALL ACCOMPLISH THIS BY DOUBLING CONES, THE USE OF SAND BAGS, RINGS OR BY OTHER MEANS, AS APPROVED BY THE ENGINEER, WHICH SHALL NOT PRESENT A HAZARD TO MOTORISTS OR WORKERS IF THE CONES, DRUMS, VERTICAL PANELS OR TUBULAR MARKERS ARE STRUCK.
28. THE CONTRACTOR SHALL BACKFILL ALL OPEN EXCAVATIONS OR PROVIDE ANCHORED STEEL PLATES TO COVER ALL TRENCH EXCAVATIONS DURING NON-WORKING HOURS. ANCHORED STEEL PLATES SHALL ALSO BE PLACED ON SUBGRADE, SUBBASE COURSES OR BASE COURSES TO PROTECT SHALLOW UTILITY FACILITIES FROM WHEEL LOADINGS DUE TO CONSTRUCTION VEHICLES AND EQUIPMENT. STEEL PLATES SHALL BE RAMPED WITH ASPHALT IN THE ROADWAY AREA TO PROVIDE A SMOOTH TRANSITION. THE COST FOR ANCHORED PLATES AND PAVEMENT SHALL BE INCLUDED IN THE PRICE BID FOR ITEM 619.01.
29. THE TEMPORARY CONCRETE BARRICADE SHALL NOT BE PLACED ALONG THE MERGING TAPER. THE LANE SHALL BE CLOSED USING CHANNELIZING DEVICES.
30. ADJUSTMENTS IN THE LOCATION OF ADVANCED WARNING SIGNS SHOULD BE MADE TO ACCOMMODATE THE HORIZONTAL AND VERTICAL ALIGNMENT OF THE ROADWAY, RECOGNIZING THAT THE DISTANCES SHOWN FOR SIGN SPACING ARE MINIMUM. ADJUSTMENTS IN THE HEIGHT OF THE SIGNAL HEADS SHALL BE MADE AS NEEDED TO CONFORM TO THE VERTICAL ALIGNMENT.
31. TRAVEL LANE WIDTH SHALL BE 16'-0" MINIMUM AT ALL TIMES UNLESS OTHERWISE DIRECTED BY THE ENGINEER.



**ON-SITE DETOUR:**

1. HOLIDAY RESTRICTIONS: IT IS ANTICIPATED THAT THE ON-SITE DETOUR WILL BE IN OPERATION OVER HOLIDAYS. NO WORK SHUTDOWN OVER HOLIDAYS IS REQUIRED.
2. THE CONTRACT PROVIDES FOR AN ON-SITE DETOUR OF CEMETERY DRIVE DURING BRIDGE CONSTRUCTION.
3. FLAGGERS ARE TO BE UTILIZED WHEN THE ON-SITE DETOUR IS UNAVAILABLE TO TRAFFIC DUE TO CONSTRUCTION ACTIVITIES. COST SHALL BE INCLUDED IN THE PRICE BID FOR ITEM 619.01.
4. PEDESTRIAN AND BICYCLE TRAFFIC WILL BE MAINTAINED ALONG THE ON-SITE DETOUR AND TEMPORARY BRIDGE.
5. THE CONTRACTOR SHALL NOTIFY MONTGOMERY COUNTY IN WRITING A MINIMUM OF 14 CALENDAR DAYS IN ADVANCE AS TO WHEN HE WISHES TO UTILIZE THE DETOUR. THE DETOUR MUST ONLY BE UTILIZED FOR THE TIME FRAMES AS OUTLINED IN THE SPECIAL NOTES "BRIDGE OPENING" AND CONFORM TO ALL SPECIAL PROVISIONS AS OUTLINED IN THE PROJECT MANUAL. THE COST OF ALL SIGNAGE AND MAINTENANCE OF THE ON-SITE DETOUR SHALL BE INCLUDED UNDER ITEM 619.01 - BASIC WORK ZONE TRAFFIC CONTROL.

**BRIDGE OPENING:**

1. CEMETERY DRIVE AND BRIDGE APPROACHES MUST BE COMPLETED IN ACCORDANCE WITH THE PROJECT MANUAL. IN THE EVENT WORK IS NOT COMPLETED TO A POINT WHERE CEMETERY DRIVE IS NOT OPENED TO TRAFFIC, LIQUIDATED DAMAGES SHALL BE ASSESSED TO THE CONTRACTOR PER TABLE 108-1 OF THE STANDARD SPECIFICATIONS.
2. PRIOR TO OPENING, THE NEW BRIDGE AND HIGHWAY APPROACHES MUST BE COMPLETED TO THE EXTENT THAT PUBLIC TRAFFIC CAN BE SAFELY ACCOMMODATED. THIS INCLUDES ALL STRUCTURAL ELEMENTS, BRIDGE AND APPROACH RAILING, AND TRAFFIC SIGNS.
3. THE CONTRACTOR AGREES IN UNDERTAKING THIS PROJECT THAT ALL CONSIDERATIONS HAVE BEEN TAKEN AND ALLOWANCES MADE FOR ALL ORDINARY DELAYS AND HINDRANCES TO SUCH WORK, SUCH AS WEATHER, SECURING MATERIALS AND LABOR, AND CONDITIONS OR RESTRICTIONS AT THE PROJECT SITES.
4. NO DIRECT PAYMENT WILL BE MADE FOR ANY WORK DESCRIBED BY THIS SPECIAL NOTE. ALL COSTS SHALL BE INCLUDED IN THE PRICE BID FOR THE VARIOUS ITEMS IN THE CONTRACT.


|   |                    |
|---|--------------------|
| AFFIX SEAL: GREENMAN - PEDERSEN, INC<br>ON:   | ALTERED BY:<br>ON: |
|  |                    |


|  |  |                    |                    |          |   |  |
|--|--|--------------------|--------------------|----------|---|--|
| AS-BUILT REVISIONS<br>DESCRIPTION OF ALTERATIONS:  | CEMETERY DRIVE OVER THE                    | PIN 2754.54        | BRIDGES<br>3310200 | CULVERTS | ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED   | CONTRACT NUMBER<br>D036212                 |
|  | SOUTH CHUCTANUNDA CREEK BRIDGE REPLACEMENT |                    |                    |          |   | WORK ZONE TRAFFIC CONTROL<br>GENERAL NOTES |
|  | TOWN OF FLORIDA                            | COUNTY: MONTGOMERY | REGION: 2          |          |   |  |
| IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION. |  |                    |                    |          |   |  |
|   |  |                    |                    |          |  |  |


PROJECT MANAGER: S. KERN  
 CHECK: S. KERN  
 DRAFTING: F. CIUCCIO  
 CHECK: B. WEAVER  
 DESIGN: S. KERN  
 JOB MANAGER: D. THOMAS  
 DESIGN SUPERVISOR: J. SIMKULET


| HORIZONTAL CONTROL AND CURVE TABLE |                |              |             |        |                         |                 |         |         |  |
|------------------------------------|----------------|--------------|-------------|--------|-------------------------|-----------------|---------|---------|--|
| H.C.L. POINT                       | H.C.L. STATION | NORTHING     | EASTING     | RADIUS | DELTA                   | AZIMUTH         | LENGTH  | TANGENT |  |
| <b>CEMETERY DRIVE DETOUR ROUTE</b> |                |              |             |        |                         |                 |         |         |  |
| POB                                | D 10+00.00     | 1473655.9984 | 569375.4248 |        |                         | 328°43'53.2414" | 34.39'  |         |  |
| PC                                 | D 10+34.39     | 1473685.3949 | 569357.5735 |        |                         |                 |         |         |  |
| PC                                 | D 10+34.39     | 1473685.3949 | 569357.5735 | 50.00' | 85°16'07.6772"<br>RIGHT |                 | 74.41'  | 46.03'  |  |
| PI                                 | D 10+80.43     | 1473724.7412 | 569333.6801 |        |                         |                 |         |         |  |
| PT                                 | D 11+08.80     | 1473751.7984 | 569370.9216 |        |                         |                 |         |         |  |
| PT                                 | D 11+08.80     | 1473751.7984 | 569370.9216 |        |                         | 54°00'00.9186"  | 102.09' |         |  |
| PC                                 | D 12+10.89     | 1473811.8054 | 569453.5149 | 50.00' | 59°12'34.8022"<br>RIGHT |                 | 51.67'  | 28.41'  |  |
| PI                                 | D 12+39.30     | 1473828.5040 | 569476.4988 |        |                         |                 |         |         |  |
| PT                                 | D 12+62.56     | 1473817.3078 | 569502.6090 |        |                         |                 |         |         |  |
| PT                                 | D 12+62.56     | 1473817.3078 | 569502.6090 |        |                         | 131°12'35.7208" | 67.04'  |         |  |
| PC                                 | D 12+10.89     | 1473811.8054 | 569453.5149 |        |                         |                 |         |         |  |
| PI                                 | D 12+39.30     | 1473828.5040 | 569476.4988 |        |                         |                 |         |         |  |
| PT                                 | D 12+62.56     | 1473817.3078 | 569502.6090 |        |                         |                 |         |         |  |
| POE                                | D 13+29.61     | 1473790.8867 | 569564.2246 |        |                         |                 |         |         |  |

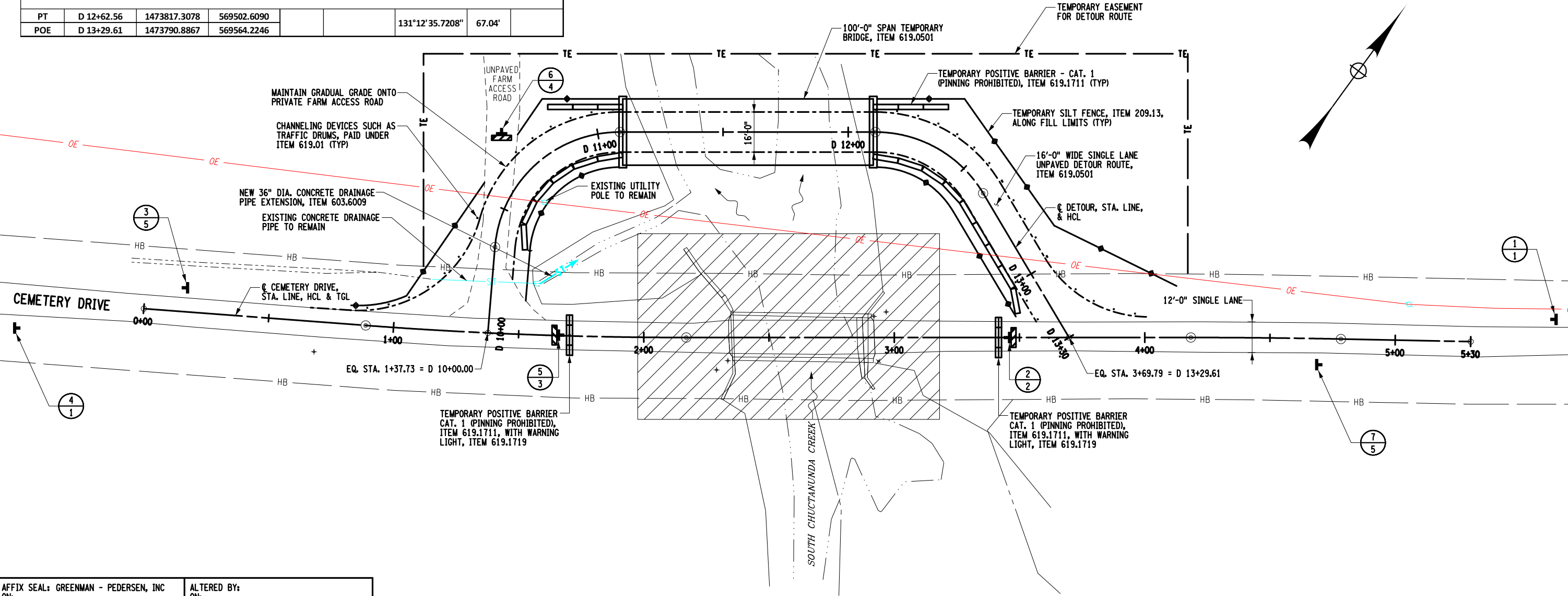
**LEGEND**

WORK ZONE 

TYPE III BARRICADE W/ WARNING LIGHTS (ITEM 619.04) 

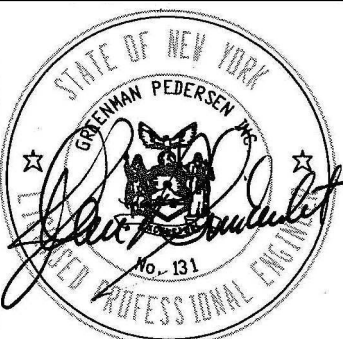
CONSTRUCTION SIGNS (ITEM 619.01) 

LOCATION NUMBER TEXT NUMBER 





**WORK ZONE TRAFFIC CONTROL PLAN**






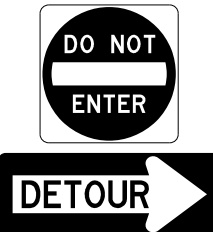
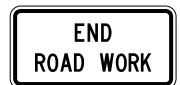
AFFIX SEAL: GREENMAN - PEDERSEN, INC  
 ON: 

ALTERED BY:  
 ON:

|  |  |             |                 |          |   |  |  |
|--|--|-------------|-----------------|----------|---|--|--|
| AS-BUILT REVISIONS<br>DESCRIPTION OF ALTERATIONS:  | CEMETERY DRIVE OVER THE                    | PIN 2754.54 | BRIDGES 3310200 | CULVERTS | ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED | CONTRACT NUMBER D036212  |  |
|  | SOUTH CHUCTANUNDA CREEK BRIDGE REPLACEMENT |             |                 |          | WORK ZONE TRAFFIC CONTROL PLAN              | DRAWING NO. WZT-2 SHEET NO. 6  |  |
|  | TOWN OF FLORIDA                            |             |                 |          |   |  |  |
|  | COUNTY: MONTGOMERY                         | REGION: 2   |                 |          |   |  |  |
| IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION. |  |             |                 |          |   |  GREENMAN-PEDERSEN, INC. CONSULTING ENGINEERS |  MONTGOMERY COUNTY DEPARTMENT OF PUBLIC WORKS |

FILE NAME = ...CADD\275454\_CPB\_WZT-2.dgn  
 DATE/TIME = 4/13/2022  
 USER = skern

## DETOUR CONSTRUCTION SIGNS

| TEXT NUMBER       | LOCATION NUMBER | TEXT  | NATIONAL OR STATE M.U.T.C.D. NUMBER |
|-------------------|-----------------|---|-------------------------------------|
| 1                 | 1, 4            |    | W20-1                               |
| 2<br>(SEE NOTE 2) | 2               |    | R11-2 (MOD.)<br><br>M4-10R          |
| 3<br>(SEE NOTE 2) | 5               |    | R11-2 (MOD.)<br><br>M4-10L          |
| 4<br>(SEE NOTE 3) | 6               |  | R5-1<br><br>M4-10R                  |
| 5                 | 3, 7            |  | G20-2                               |

**NOTES:**

1. ALL DETOUR SIGNS PAID FOR UNDER ITEM 619.01.
2. SIGNS WITH TEXT 2 AND 3 ARE TO BE MOUNTED ON A TYPE III BARRICADE LOCATED ON EITHER SIDE OF THE WORK ZONE.
3. SIGNS WITH TEXT 4 ARE TO BE MOUNTED ON A TYPE III BARRICADE LOCATED ON THE FARM ACCESS ROAD AS SHOWN ON DWG. WZT-2. THE BARRICADE IS TO BE LOCATED AT THE EDGE OF TRAVELED WAY TO ALLOW FOR PRIVATE TRAFFIC.

PROJECT MANAGER S. KERN

CHECK S. KERN

DRAFTING F. CIUCCIO


CHECK B. WEAVER

DESIGN S. KERN

JOB MANAGER D. THOMAS

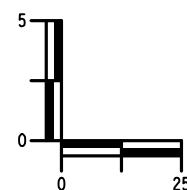
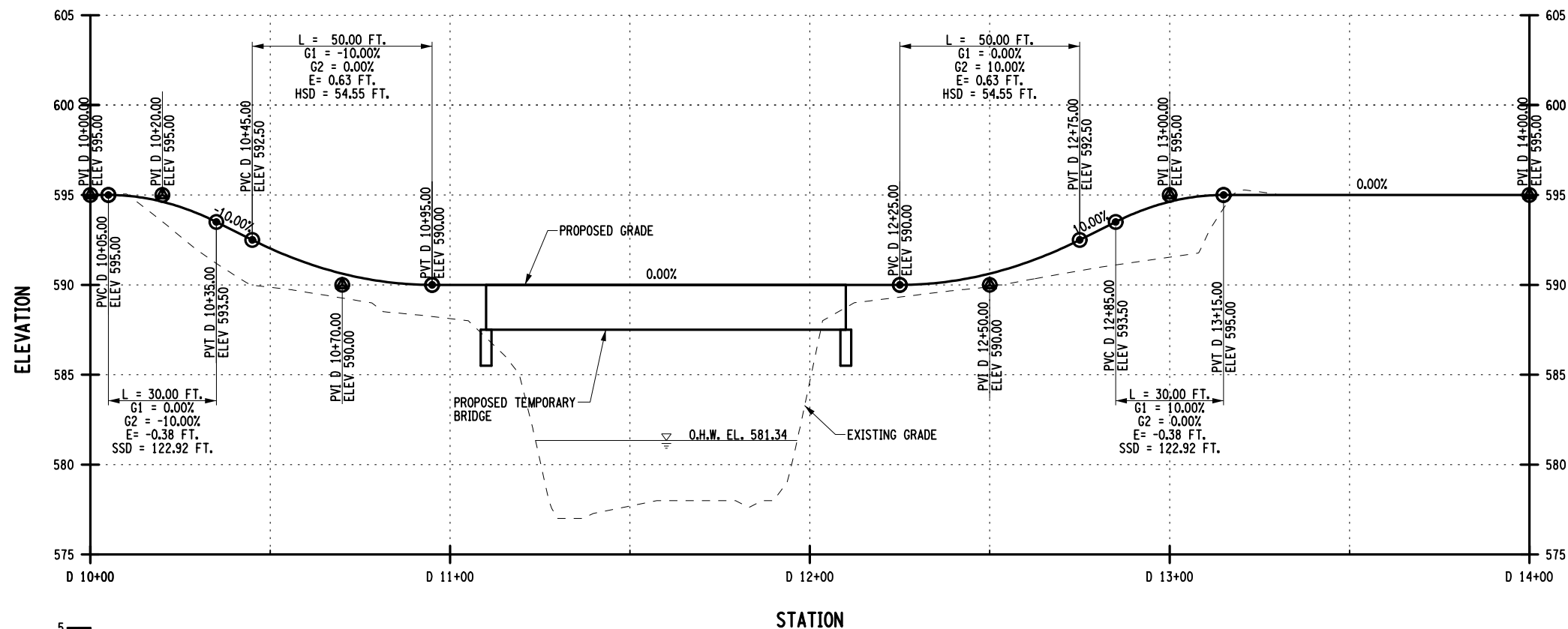
DESIGN SUPERVISOR J. SIMKULET

FILE NAME = ...CAD0275454.CPB.WZT-3.dgn  
DATE/TIME = 4/13/2022  
USER = skern

|   |                    |
|---|--------------------|
| AFFIX SEAL: GREENMAN - PEDERSEN, INC<br>ON:  | ALTERED BY:<br>ON: |
|---|--------------------|

|  |   |             |                    |          |   |                                  |
|--|---|-------------|--------------------|----------|---|----------------------------------|
| AS-BUILT REVISIONS<br>DESCRIPTION OF ALTERATIONS:  | CEMETERY DRIVE OVER THE<br>SOUTH CHUCTANUNDA CREEK BRIDGE REPLACEMENT | PIN 2754.54 | BRIDGES<br>3310200 | CULVERTS | ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED     | CONTRACT NUMBER<br>D036212       |
| IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION. |   |             |                    |          | WORK ZONE TRAFFIC CONTROL<br>SIGN TABLE         | DRAWING NO. WZT-3<br>SHEET NO. 7 |
| GREENMAN-PEDERSEN, INC. CONSULTING ENGINEERS   |   |             |                    |          | MONTGOMERY COUNTY<br>DEPARTMENT OF PUBLIC WORKS |                                  |

CEMETERY DRIVE DETOUR ROUTE



**NOTES**  
 1. THE COST OF ALL EXCAVATION, FILL, AND GRADING FOR THE DETOUR ROUTE IS TO BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 619.0501.

| HYDRAULIC DATA TABLE                         |                      |             |              |
|--|----------------------|-------------|--------------|
| DRAINAGE AREA =                              | 21.20 (SQ. MILES)    | BASIC FLOOD | DESIGN FLOOD |
| REQUENCY INTERVAL                            | (YRS.)               | 100         | 50           |
| PEAK DISCHARGE                               | (FT <sup>3</sup> /S) | 2400        | 2076         |
| HIGH WATER ELEVATION @ PT. OF MAX. BACKWATER | EXISTING             | 584.48      | 583.93       |
|  | PROPOSED             | 584.35      | 583.80       |
| AVG. VELOCITY THRU STRUCTURE @ DESIGN FLOOD  | (FT/S)               | 7.07        | 6.84         |

THE PROPOSED TEMPORARY STRUCTURE SHALL PROVIDE A MINIMUM CLEAR OPENING OF 80 FT. PERPENDICULAR TO THE FLOW WITH A MINIMUM ACCEPTABLE LOW BEAM ELEVATION OF 586.50. A MINIMUM CLEAR WATERWAY AREA OF 3,600 SQUARE FEET IS REQUIRED BELOW THE MINIMUM LOW BEAM ELEVATION.

AFFIX SEAL: GREENMAN - PEDERSEN, INC  
 ON:

ALTERED BY:  
 ON:

|  |  |             |                    |          |   |   |   |
|--|--|-------------|--------------------|----------|---|---|---|
| AS-BUILT REVISIONS<br>DESCRIPTION OF ALTERATIONS:  | CEMETERY DRIVE OVER THE                    | PIN 2754.54 | BRIDGES<br>3310200 | CULVERTS | ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED | CONTRACT NUMBER<br>D036212                      |   |
|  | SOUTH CHUCTANUNDA CREEK BRIDGE REPLACEMENT |             |                    |          |   | DETOUR PROFILE                                  | DRAWING NO. WZT-4<br>SHEET NO. 8                |
|  | TOWN OF FLORIDA                            |             |                    |          |   |   |   |
|  | COUNTY: MONTGOMERY                         | REGION: 2   |                    |          |   |   |   |
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PROJECT MANAGER: S. KERN  
 CHECK: S. KERN  
 DRAFTING: F. CIUCCIO  
 CHECK: B. WEAVER  
 DESIGN: S. KERN  
 JOB MANAGER: D. THOMAS  
 DESIGN SUPERVISOR: J. SIMKULET  
 FILE NAME: ...\\CADD\275454\_CPB\_WZT-4.dgn  
 DATE/TIME: 4/13/2022  
 USER: skern



FILE NAME = \\CADD\275454\_CPB\_GEN.PLAN.dgn  
 DATE/TIME = 4/13/2022  
 USER = skern

DESIGN SUPERVISOR J. SIMKULET

JOB MANAGER D. THOMAS

DESIGN S. KERN

CHECK B. WEAVER

DRAFTING F. CIUCCIO

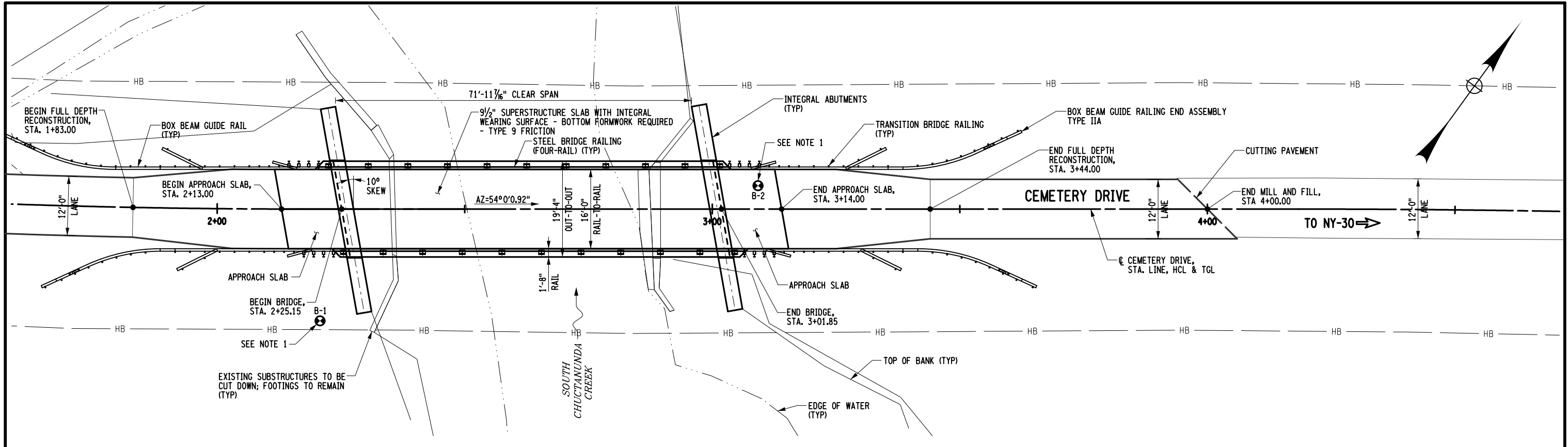
CHECK S. KERN

PROJECT MANAGER S. KERN

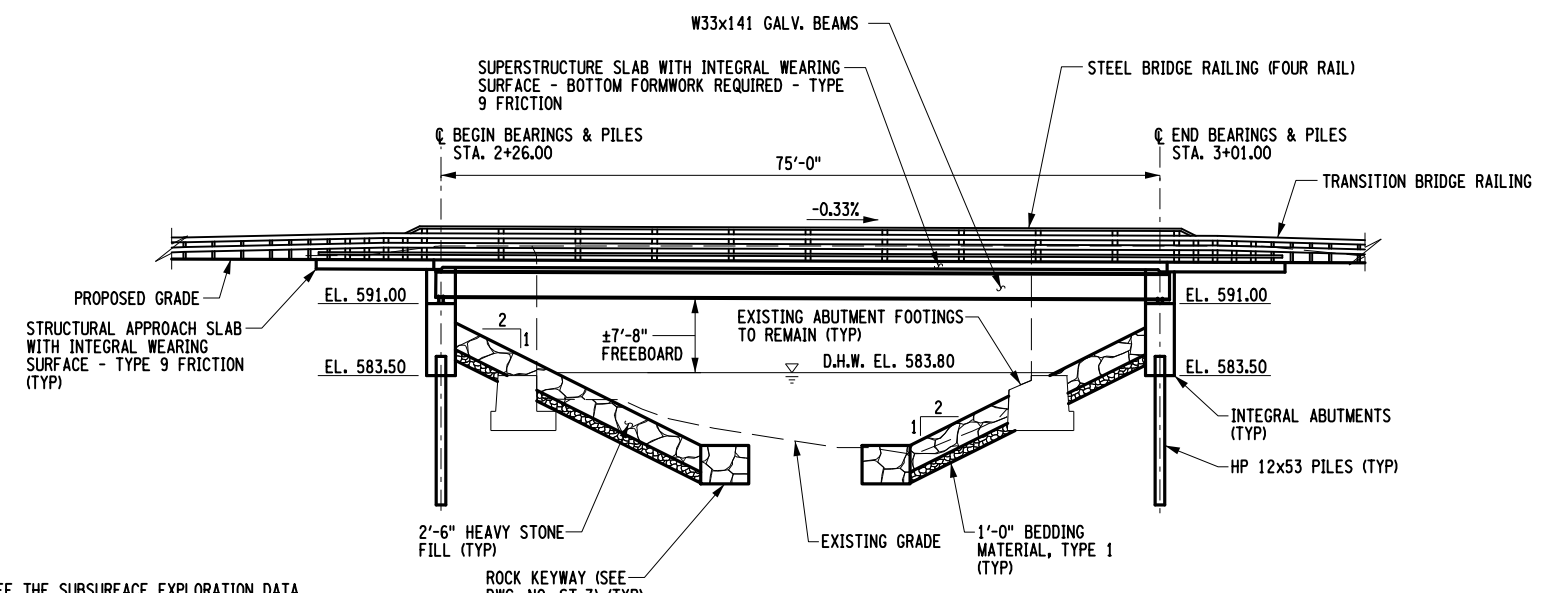
| HORIZONTAL CONTROL AND CURVE TABLE |                |              |             |          |               |                |         |         |
|------------------------------------|----------------|--------------|-------------|----------|---------------|----------------|---------|---------|
| H.C.L. POINT                       | H.C.L. STATION | NORTHING     | EASTING     | RADIUS   | DELTA         | AZIMUTH        | LENGTH  | TANGENT |
| <b>CEMETERY DRIVE</b>              |                |              |             |          |               |                |         |         |
| POB                                | 0+00.00        | 1473583.0674 | 569258.5948 |          |               | 58°19'04.4479" | 88.85'  |         |
| PC                                 | 0+88.85        | 1473629.7312 | 569334.2028 |          |               |                |         |         |
| PT                                 | 2+16.96        | 1473701.0558 | 569440.5818 | 1700.00' | 4°19'03.5293" |                | 128.11' | 64.08'  |
| PI                                 | 1+52.93        | 1473663.3884 | 569388.7366 |          | LEFT          |                |         |         |
| PC                                 | 0+88.85        | 1473629.7312 | 569334.2028 |          |               |                |         |         |
| PT                                 | 2+16.96        | 1473701.0558 | 569440.5818 |          |               | 54°00'00.9186" | 201.45' |         |
| PI                                 | 1+52.93        | 1473663.3884 | 569388.7366 |          |               |                |         |         |
| PC                                 | 0+88.85        | 1473629.7312 | 569334.2028 |          |               |                |         |         |
| PT                                 | 2+16.96        | 1473701.0558 | 569440.5818 |          |               |                |         |         |
| PI                                 | 1+52.93        | 1473663.3884 | 569388.7366 | 3000.00' | 1°08'39.2972" |                | 59.91'  | 29.96'  |
| PC                                 | 4+18.41        | 1473819.4650 | 569603.5596 |          |               |                |         |         |
| PT                                 | 4+78.32        | 1473854.1943 | 569652.3786 |          |               |                |         |         |
| PI                                 | 4+48.36        | 1473837.0734 | 569627.7957 |          |               |                |         |         |
| PC                                 | 4+18.41        | 1473819.4650 | 569603.5596 |          |               |                |         |         |
| PT                                 | 4+78.32        | 1473854.1943 | 569652.3786 |          |               | 55°08'40.2158" | 51.84'  |         |
| PI                                 | 4+48.36        | 1473837.0734 | 569627.7957 |          |               |                |         |         |
| PC                                 | 4+18.41        | 1473819.4650 | 569603.5596 |          |               |                |         |         |
| PT                                 | 4+78.32        | 1473854.1943 | 569652.3786 |          |               |                |         |         |
| PI                                 | 4+48.36        | 1473837.0734 | 569627.7957 |          |               |                |         |         |
| PC                                 | 4+18.41        | 1473819.4650 | 569603.5596 |          |               |                |         |         |
| PT                                 | 4+78.32        | 1473854.1943 | 569652.3786 |          |               |                |         |         |
| PI                                 | 4+48.36        | 1473837.0734 | 569627.7957 |          |               |                |         |         |
| PC                                 | 4+18.41        | 1473819.4650 | 569603.5596 |          |               |                |         |         |
| PT                                 | 4+78.32        | 1473854.1943 | 569652.3786 |          |               |                |         |         |
| PI                                 | 4+48.36        | 1473837.0734 | 569627.7957 |          |               |                |         |         |
| PC                                 | 4+18.41        | 1473819.4650 | 569603.5596 |          |               |                |         |         |
| PT                                 | 4+78.32        | 1473854.1943 | 569652.3786 |          |               |                |         |         |
| PI                                 | 4+48.36        | 1473837.0734 | 569627.7957 |          |               |                |         |         |
| PC                                 | 4+18.41        | 1473819.4650 | 569603.5596 |          |               |                |         |         |
| PT                                 | 4+78.32        | 1473854.1943 | 569652.3786 |          |               |                |         |         |
| PI                                 | 4+48.36        | 1473837.0734 | 569627.7957 |          |               |                |         |         |
| PC                                 | 4+18.41        | 1473819.4650 | 569603.5596 |          |               |                |         |         |
| PT                                 | 4+78.32        | 1473854.1943 | 569652.3786 |          |               |                |         |         |
| PI                                 | 4+48.36        | 1473837.0734 | 569627.7957 |          |               |                |         |         |
| PC                                 | 4+18.41        | 1473819.4650 | 569603.5596 |          |               |                |         |         |
| PT                                 | 4+78.32        | 1473854.1943 | 569652.3786 |          |               |                |         |         |
| PI                                 | 4+48.36        | 1473837.0734 | 569627.7957 |          |               |                |         |         |
| PC                                 | 4+18.41        | 1473819.4650 | 569603.5596 |          |               |                |         |         |
| PT                                 | 4+78.32        | 1473854.1943 | 569652.3786 |          |               |                |         |         |
| PI                                 | 4+48.36        | 1473837.0734 | 569627.7957 |          |               |                |         |         |
| PC                                 | 4+18.41        | 1473819.4650 | 569603.5596 |          |               |                |         |         |
| PT                                 | 4+78.32        | 1473854.1943 | 569652.3786 |          |               |                |         |         |
| PI                                 | 4+48.36        | 1473837.0734 | 569627.7957 |          |               |                |         |         |
| PC                                 | 4+18.41        | 1473819.4650 | 569603.5596 |          |               |                |         |         |
| PT                                 | 4+78.32        | 1473854.1943 | 569652.3786 |          |               |                |         |         |
| PI                                 | 4+48.36        | 1473837.0734 | 569627.7957 |          |               |                |         |         |
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| PI                                 | 4+48.36        | 1473837.0734 | 569627.7957 |          |               |                |         |         |
| PC                                 | 4+18.41        | 1473819.4650 | 569603.5596 |          |               |                |         |         |
| PT                                 | 4+78.32        | 1473854.1943 | 569652.3786 |          |               |                |         |         |
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| PC                                 | 4+18.41        | 1473819.4650 | 569603.5596 |          |               |                |         |         |
| PT                                 | 4+78.32        | 1473854.1943 | 5           |          |               |                |         |         |

PROJECT MANAGER S. KERN  
 CHECK S. KERN  
 DRAFTING F. CUOCCIO  
 CHECK B. WEAVER  
 DESIGN S. KERN  
 JOB MANAGER D. THOMAS  
 DESIGN SUPERVISOR J. SIMKULET

FILE NAME = ...CADD\275454\_CPB\_BRD\_PLN.P.dgn  
 DATE/TIME = 4/13/2022  
 USER = skern



PLAN



ELEVATION

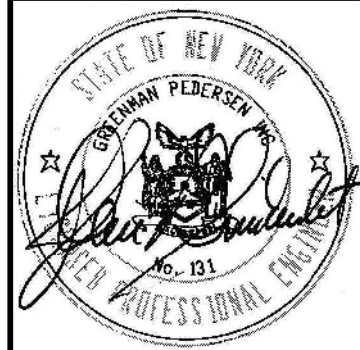
| HYDRAULIC DATA TABLE                                    |                                    |                                    |                                    |
|---|------------------------------------|------------------------------------|------------------------------------|
| DRAINAGE AREA =   | 21.20 (SQ. MILES)                  | BASIC FLOOD                        | DESIGN FLOOD                       |
| RECURRENTY INTERVAL                                     | (YRS.)                             | 100                                | 50                                 |
| PEAK DISCHARGE  | (FT <sup>3</sup> /S)               | 2400                               | 2076                               |
| HIGH WATER ELEVATION @ PT. OF MAX. BACKWATER            | EXISTING                           | 584.48                             | 583.93                             |
|   | PROPOSED                           | 584.35                             | 583.80                             |
| AVG. VELOCITY THRU STRUCTURE @ DESIGN FLOOD             | (FT/S)                             | 7.07                               | 6.84                               |
| SCOUR ANALYSIS:   | MINIMUM CHANNEL EL. 574.82 FT      |                                    |                                    |
|   | Q <sub>100</sub> SCOUR DEPTH (FT.) | Q <sub>100</sub> SCOUR ELEV. (FT.) | Q <sub>500</sub> SCOUR DEPTH (FT.) |
| BEGIN ABUTMENT  | 2.91                               | 571.91                             | 2.40                               |
| END ABUTMENT  | 1.94                               | 572.88                             | 0.67                               |
| SCOUR DEPTH IS MEASURED FROM MINIMUM CHANNEL ELEVATION. |                                    |                                    |                                    |

| LOAD FACTOR (LFD) RATING |                   |
|--------------------------|-------------------|
| INVENTORY                | HS-38<br>69 TONS  |
| OPERATING                | HS-64<br>115 TONS |

| LRFD RATING FACTORS |      |
|---------------------|------|
| INVENTORY           | 1.81 |
| OPERATING           | 2.34 |

**NOTES:**  
 1. FOR BORING DATA, SEE THE SUBSURFACE EXPLORATION DATA REPORT INCLUDED AS SUPPLEMENTAL INFORMATION IN THE PROJECT MANUAL.

AFFIX SEAL: GREENMAN - PEDERSEN, INC  
 ON: \_\_\_\_\_  
 ALTERED BY:  
 ON: \_\_\_\_\_



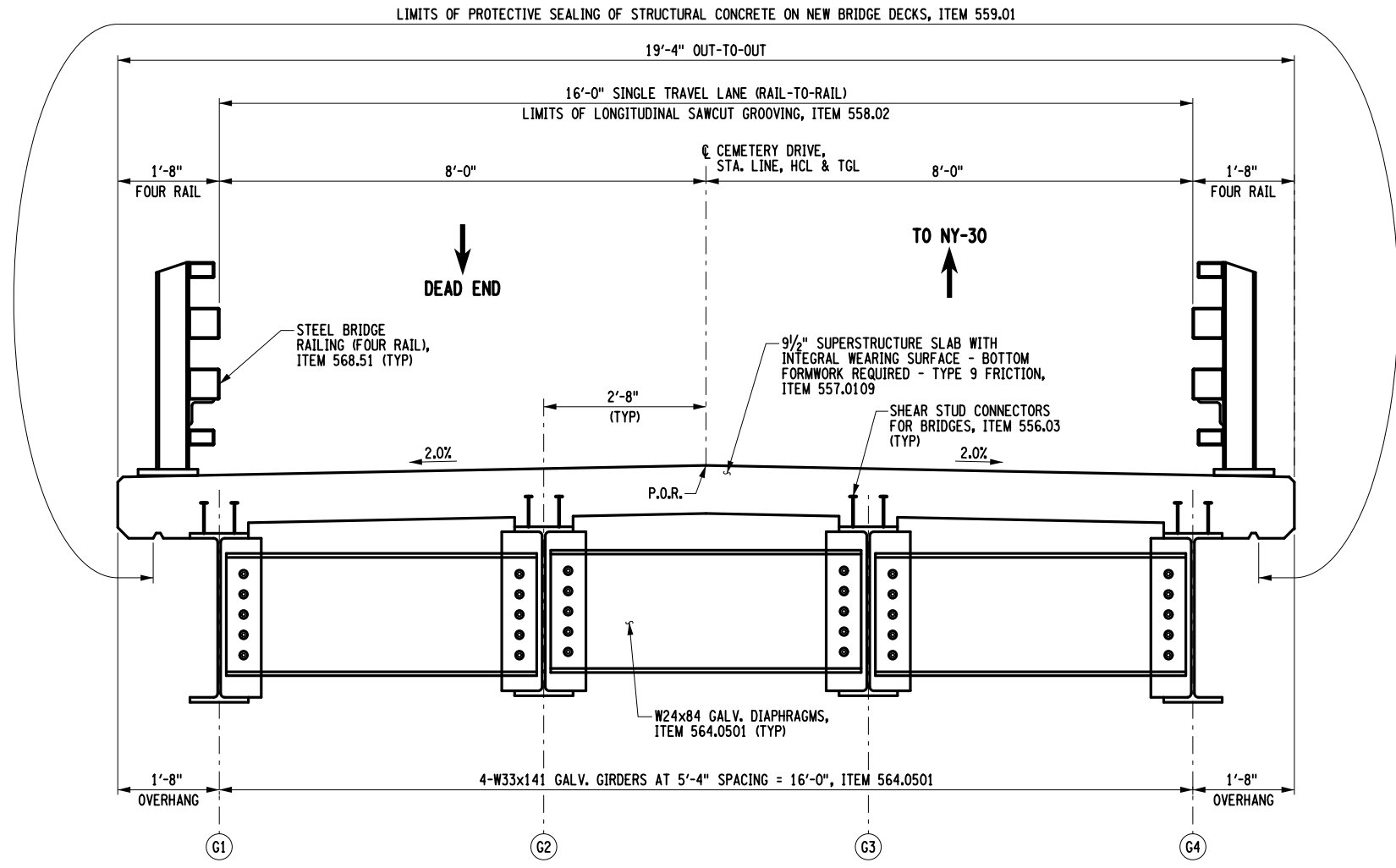
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|---|--|-------------|--------------------|----------|---|----------------------------------|
| AS-BUILT REVISIONS<br>DESCRIPTION OF ALTERATIONS: | CEMETERY DRIVE OVER THE                    | PIN 2754.54 | BRIDGES<br>3310200 | CULVERTS | ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED | CONTRACT NUMBER<br>D036212       |
|   | SOUTH CHUCTANUNDA CREEK BRIDGE REPLACEMENT |             |                    |          | BRIDGE PLAN AND ELEVATION                   | DRAWING NO. GP-2<br>SHEET NO. 10 |
|   | TOWN OF FLORIDA                            |             |                    |          |   |                                  |
|   | COUNTY: MONTGOMERY                         | REGION: 2   |                    |          |   |                                  |

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

**GPI** GREENMAN-PEDERSEN, INC. CONSULTING ENGINEERS  
 MONTGOMERY COUNTY DEPARTMENT OF PUBLIC WORKS

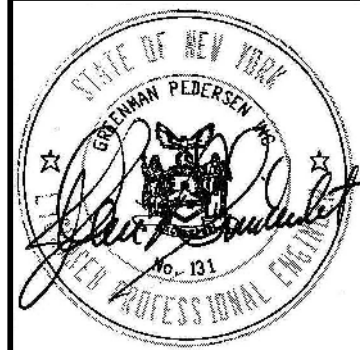
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 DATE/TIME = 4/13/2022  
 USER = skern

DESIGN SUPERVISOR J. SIMKULET  
 JOB MANAGER D. THOMAS  
 DESIGN S. KERN  
 CHECK B. WEAVER  
 DRAFTING F. CIUCCIO  
 CHECK S. KERN  
 PROJECT MANAGER S. KERN



**TYPICAL BRIDGE SECTION**  
 SCALE: 3/8"=1'-0"

AFFIX SEAL: GREENMAN - PEDERSEN, INC  
 ON: \_\_\_\_\_  
 ALTERED BY:  
 ON: \_\_\_\_\_



|   |  |             |                 |          |   |                            |
|---|--|-------------|-----------------|----------|---|----------------------------|
| AS-BUILT REVISIONS<br>DESCRIPTION OF ALTERATIONS: | CEMETERY DRIVE OVER THE                    | PIN 2754.54 | BRIDGES 3310200 | CULVERTS | ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED | CONTRACT NUMBER<br>D036212 |
|   | SOUTH CHUCTANUNDA CREEK BRIDGE REPLACEMENT |             |                 |          |   | TYPICAL BRIDGE SECTION     |
|   | TOWN OF FLORIDA                            |             |                 |          |   |                            |
|   | COUNTY: MONTGOMERY                         | REGION: 2   |                 |          |   |                            |

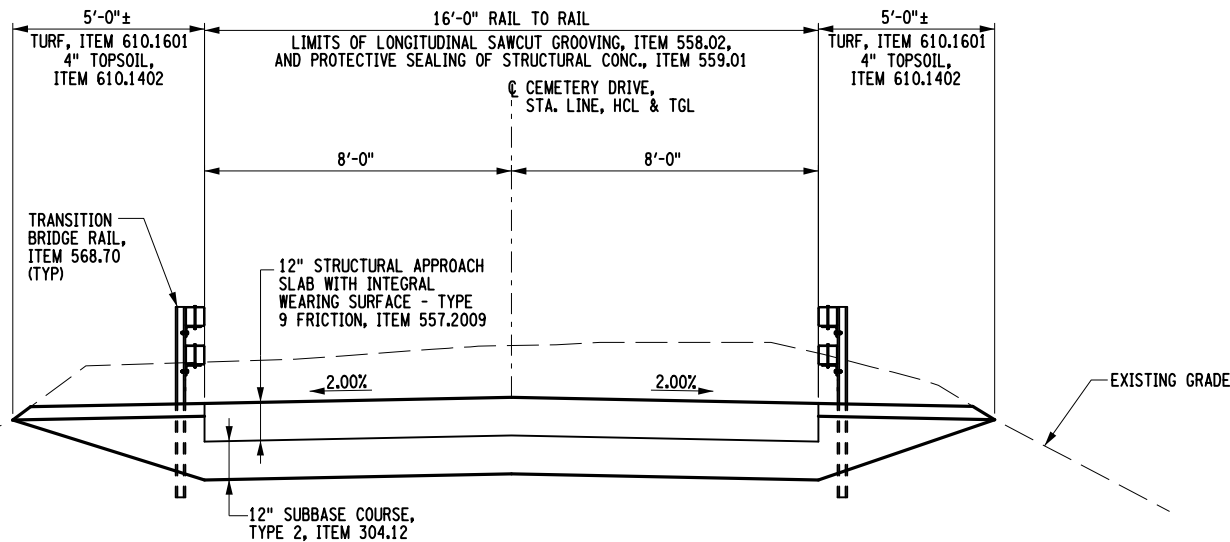
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**GPI** GREENMAN-PEDERSEN, INC. CONSULTING ENGINEERS

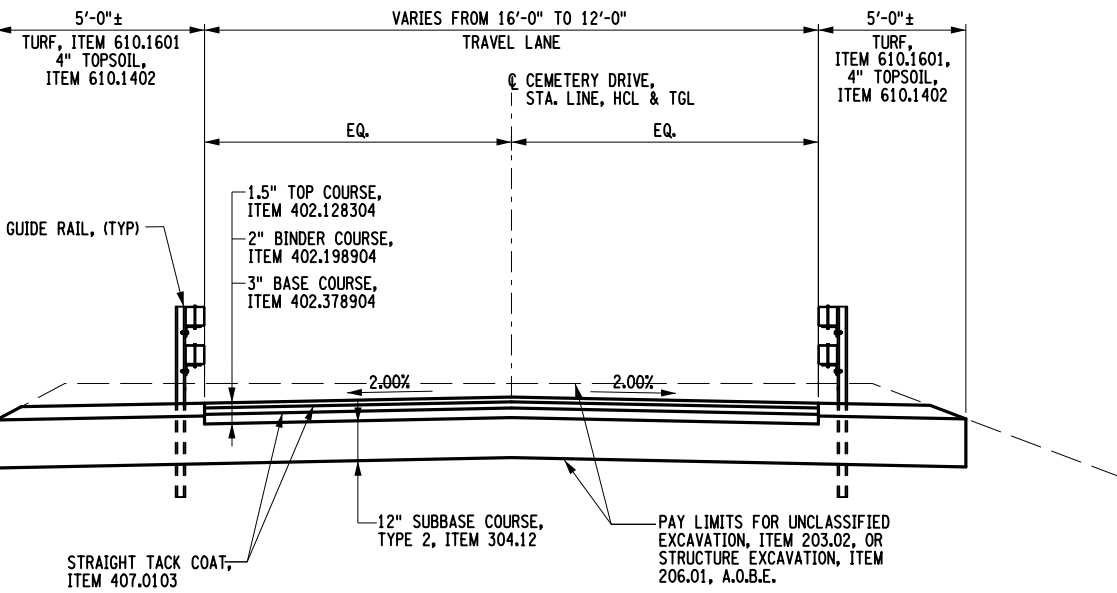
MONTGOMERY COUNTY DEPARTMENT OF PUBLIC WORKS

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 USER = skern

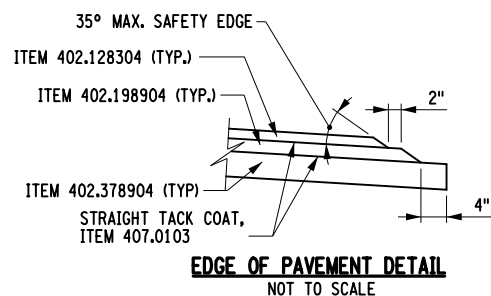
DESIGN SUPERVISOR J. SIMKULET  
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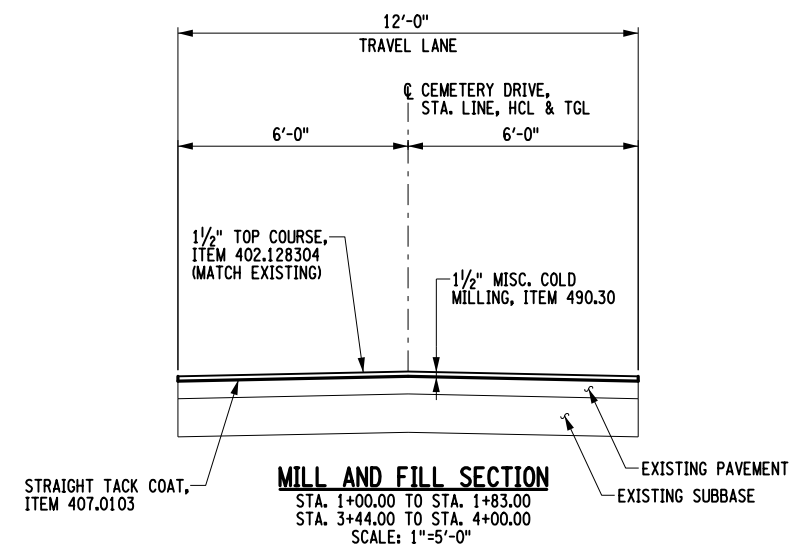
**APPROACH SLAB SECTION**  
 STA. 2+13.00 TO STA. 2+25.15  
 STA. 3+01.85 TO STA. 3+14.00  
 (STA. 2+25.00 SHOWN)  
 SCALE: 1"=5'-0"



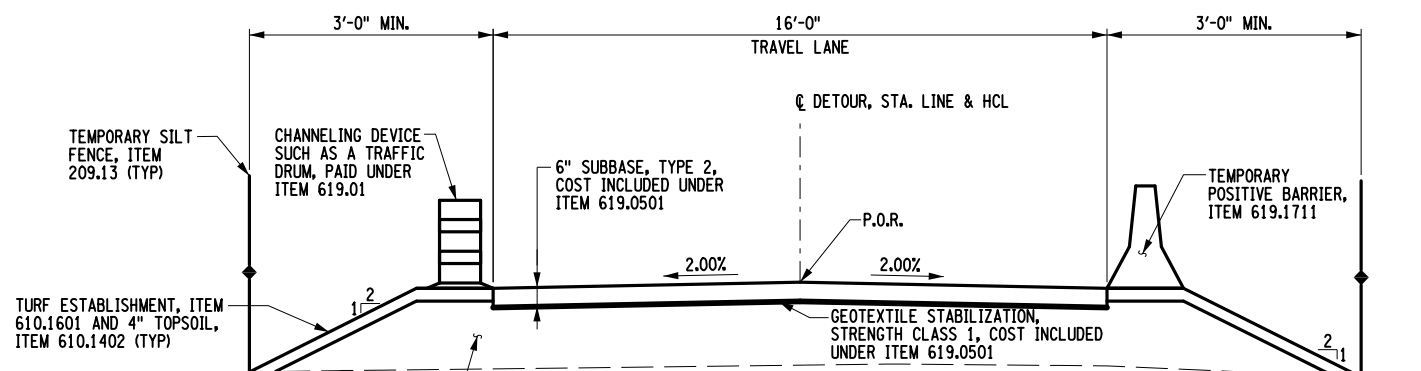
**FULL DEPTH RECONSTRUCTION SECTION**  
 STA. 1+83.00 TO STA. 2+13.00  
 STA. 3+14.00 TO STA. 3+44.00  
 (STA. 2+00.00 SHOWN)  
 SCALE: 1"=5'-0"



**EDGE OF PAVEMENT DETAIL**  
 NOT TO SCALE

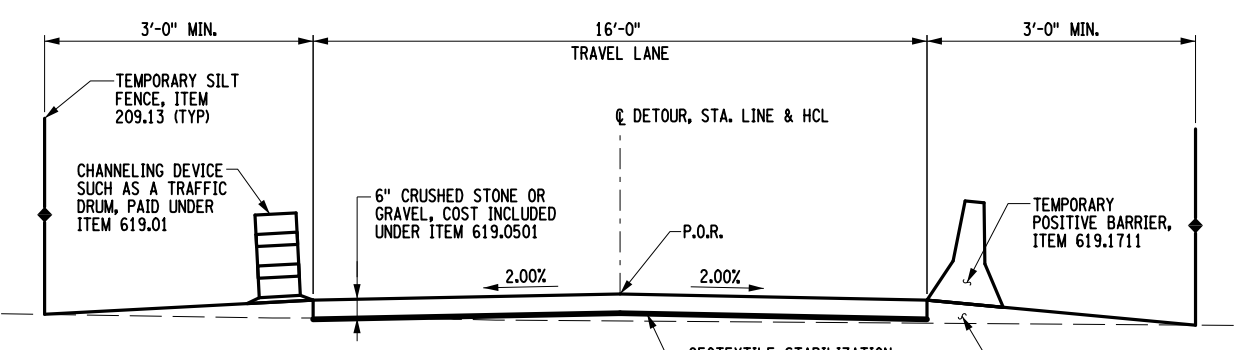


**MILL AND FILL SECTION**  
 STA. 1+00.00 TO STA. 1+83.00  
 STA. 3+44.00 TO STA. 4+00.00  
 SCALE: 1"=5'-0"



**TYPICAL SOUTH DETOUR APPROACH SECTION**  
 (STA D 10+50.00 SHOWN)  
 SCALE: 1"=5'-0"

NOTE: ALL FILL MATERIAL AND GRADING IMPROVEMENTS TO REMAIN AT PROJECT COMPLETION.



**TYPICAL NORTH DETOUR APPROACH SECTION**  
 (STA D 12+50.00 SHOWN)  
 SCALE: 1"=5'-0"

NOTE: AT THE END OF CONSTRUCTION ACTIVITIES, ALL FILL MATERIAL USED TO CONSTRUCT THE NORTH DETOUR APPROACH IS TO BE REMOVED AND STOCKPILED FOR THE PROPERTY OWNER AT THEIR DISCRETION. THE NORTH DETOUR APPROACH IS TO BE REESTABLISHED AS FARMLAND. COST INCLUDED UNDER ITEM 619.0501.

AFFIX SEAL: GREENMAN - PEDERSEN, INC ON:

ALTERED BY: ON:

|                             |  |             |                 |          |   |                         |
|-----------------------------|--|-------------|-----------------|----------|---|-------------------------|
| AS-BUILT REVISIONS          | CEMETERY DRIVE OVER THE                    | PIN 2754.54 | BRIDGES 3310200 | CULVERTS | ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED | CONTRACT NUMBER D036212 |
| DESCRIPTION OF ALTERATIONS: | SOUTH CHUCTANUNDA CREEK BRIDGE REPLACEMENT |             |                 |          | TYPICAL ROADWAY SECTIONS                    | DRAWING NO. TS-2        |
|                             | TOWN OF FLORIDA                            |             |                 |          |   | SHEET NO. 12            |
|                             | COUNTY: MONTGOMERY                         | REGION: 2   |                 |          |   |                         |

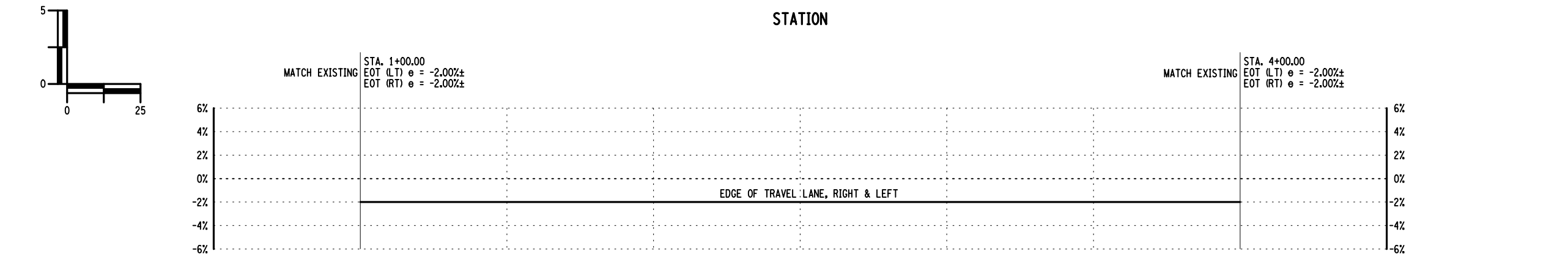
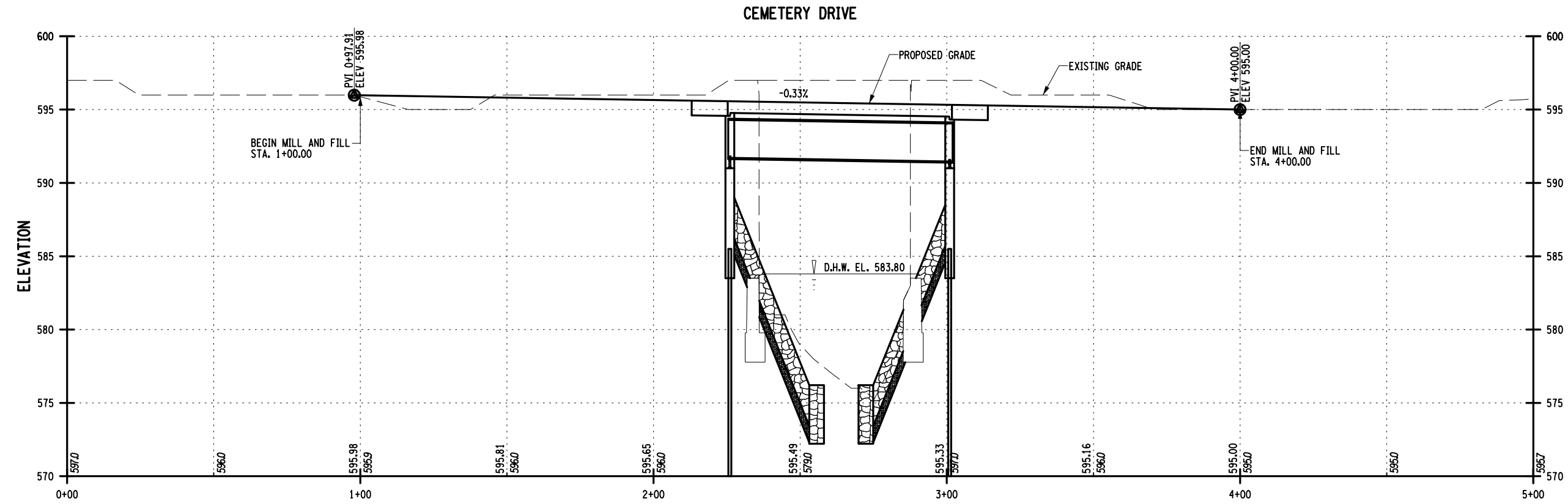
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**GPI** GREENMAN-PEDERSEN, INC. CONSULTING ENGINEERS

MONTGOMERY COUNTY DEPARTMENT OF PUBLIC WORKS

PROJECT MANAGER S. KERN  
 CHECK S. KERN  
 DRAFTING F. CIUCCIO  
 CHECK B. WEAVER  
 DESIGN S. KERN  
 JOB MANAGER D. THOMAS  
 DESIGN SUPERVISOR J. SIMKULET

FILE NAME = \\CADD\275454\_CPB\_PRO.dgn  
 DATE/TIME = 4/13/2022  
 USER = skern



AFFIX SEAL: GREENMAN - PEDERSEN, INC  
 ON: \_\_\_\_\_  
 ALTERED BY:  
 ON: \_\_\_\_\_



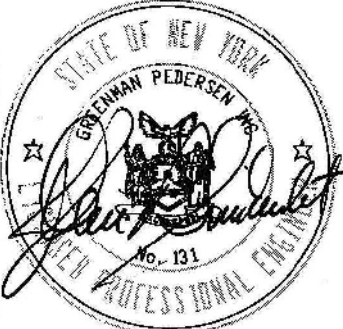
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|---|--|-------------|--------------------|----------|---|-----------------------------------|
| AS-BUILT REVISIONS<br>DESCRIPTION OF ALTERATIONS: | CEMETERY DRIVE OVER THE                    | PIN 2754.54 | BRIDGES<br>3310200 | CULVERTS | ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED | CONTRACT NUMBER<br>D036212        |
|   | SOUTH CHUCTANUNDA CREEK BRIDGE REPLACEMENT |             |                    |          |   | DRAWING NO. PRO-1<br>SHEET NO. 13 |
|   | TOWN OF FLORIDA                            |             |                    |          | PROFILE                                     |                                   |
|   | COUNTY: MONTGOMERY                         | REGION: 2   |                    |          |   |                                   |


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 MONTGOMERY COUNTY DEPARTMENT OF PUBLIC WORKS



| ESTIMATE OF QUANTITIES |  |      |          |       |
|------------------------|--|------|----------|-------|
| ITEM NO.               | DESCRIPTION  | UNIT | ESTIMATE | FINAL |
| 201.06                 | CLEARING AND GRUBBING  | LS   | 1        |       |
| 202.120001             | REMOVING EXISTING SUPERSTRUCTURES  | LS   | 1        |       |
| 202.19                 | REMOVAL OF SUBSTRUCTURES   | CY   | 75       |       |
| 203.02                 | UNCLASSIFIED EXCAVATION AND DISPOSAL                                     | CY   | 35       |       |
| 203.03                 | EMBANKMENT IN PLACE  | CY   | 395      |       |
| 203.21                 | SELECT STRUCTURAL FILL   | CY   | 120      |       |
| 206.01                 | STRUCTURE EXCAVATION   | CY   | 1,000    |       |
| 206.0201               | TRENCH AND CULVERT EXCAVATION  | CY   | 215      |       |
| 207.27                 | PREFABRICATED COMPOSITE INTEGRAL ABUTMENT DRAIN                          | SY   | 100      |       |
| 209.13                 | SILT FENCE - TEMPORARY   | LF   | 450      |       |
| 304.12                 | SUBBASE COURSE, TYPE 2   | CY   | 55       |       |
| 402.128304             | 12.5 F3 TOP COURSE HMA, 80 SERIES COMPACTION                             | TON  | 26       |       |
| 402.198904             | 19 F9 BINDER COURSE HMA, 80 SERIES COMPACTION                            | TON  | 13       |       |
| 402.378904             | 37.5 F9 BASE COURSE HMA, 80 SERIES COMPACTION                            | TON  | 19       |       |
| 407.0103               | STRAIGHT TACK COAT   | GAL  | 21       |       |
| 490.30                 | MISCELLANEOUS COLD MILLING OF BITUMINOUS CONCRETE                        | SY   | 195      |       |
| 551.012053             | STEEL H-PILES (HP 12X53)   | LF   | 370      |       |
| 551.13                 | FURNISHING EQUIPMENT FOR DRIVING PILES                                   | LS   | 1        |       |
| 553.020001             | COFFERDAM (TYPE 2)   | EA   | 1        |       |
| 553.020002             | COFFERDAM (TYPE 2)   | EA   | 1        |       |
| 555.09                 | CONCRETE FOR STRUCTURES, CLASS HP  | CY   | 106      |       |
| 556.0203               | GALVANIZED BAR REINFORCEMENT FOR STRUCTURES                              | LB   | 20,900   |       |
| 556.03                 | STUD SHEAR CONNECTORS FOR BRIDGES  | EA   | 560      |       |
| 557.0109               | SUPERSTRUCTURE SLAB WITH INTEGRAL WEARING SURFACE - TYPE 9 FRICTION      | SY   | 165      |       |
| 557.2009               | STRUCTURAL APPROACH SLAB WITH INTEGRAL WEARING SURFACE - TYPE 9 FRICTION | SY   | 46       |       |
| 558.02                 | LONGITUDINAL SAWCUT GROOVING OF STRUCTURAL SLAB SURFACE                  | SY   | 180      |       |
| 559.01                 | PROTECTIVE SEALING OF STRUCTURAL CONCRETE ON NEW BRIDGE DECKS            | SF   | 1,805    |       |
| 564.0501               | STRUCTURAL STEEL, TYPE 1   | LS   | 1        |       |
| 564.20010008           | HOT-DIP GALVANIZING OF STRUCTURAL STEEL                                  | LB   | 49,400   |       |
| 568.51                 | STEEL BRIDGE RAILING (FOUR RAIL)   | LF   | 166      |       |
| 568.70                 | TRANSITION BRIDGE RAILING  | LF   | 128      |       |
| 603.6009               | REINFORCED CONCRETE PIPE CLASS III, 36 INCH DIAMETER                     | LF   | 25       |       |
| 606.10                 | BOX BEAM GUIDE RAILING   | LF   | 18       |       |
| 606.120201             | BOX BEAM GUIDE RAILING END ASSEMBLY, TYPE IIA                            | EA   | 4        |       |
| 610.1402               | TOPSOIL - ROADSIDE   | CY   | 26       |       |
| 610.1601               | TURF ESTABLISHMENT - ROADSIDE  | SY   | 235      |       |
| 619.01                 | BASIC WORK ZONE TRAFFIC CONTROL  | LS   | 1        |       |
| 619.04                 | TYPE III CONSTRUCTION BARRICADES WITH LIGHTS                             | EA   | 3        |       |
| 619.0501               | TEMPORARY STRUCTURES AND APPROACHES NO 1                                 | LS   | 1        |       |
| 619.1711               | TEMPORARY POSITIVE BARRIER - CATEGORY 1 (PINNING PROHIBITED)             | LF   | 250      |       |
| 619.1719               | WARNING LIGHTS ON TEMPORARY POSITIVE BARRIERS                            | EA   | 2        |       |
| 620.04                 | STONE FILLING (MEDIUM)   | CY   | 90       |       |
| 620.05                 | STONE FILLING (HEAVY)  | CY   | 660      |       |
| 620.0801               | BEDDING MATERIAL, TYPE 1   | CY   | 210      |       |
| 625.01                 | SURVEY OPERATIONS  | LS   | 1        |       |
| 627.50140008           | CUTTING PAVEMENT   | LF   | 58       |       |
| 637.11                 | ENGINEER'S FIELD OFFICE - TYPE 1   | MNTH | 5        |       |
| 646.23                 | LARGE SNOWPLOWING DELINEATOR   | EA   | 16       |       |
| 646.32                 | STEEL POST, 2.0 LB/FT  | EA   | 8        |       |
| 647.61                 | REMOVE AND DISPOSE SIGNS, GROUND MOUNTED - SIZE I (UNDER 30 SF)          | EA   | 5        |       |
| 697.03                 | FIELD CHANGE PAYMENT   | DC   | 66,000   |       |
| 698.04                 | ASPHALT PRICE ADJUSTMENT   | DC   | 100      |       |
| 698.05                 | FUEL PRICE ADJUSTMENT  | DC   | 250      |       |
| 698.06                 | STEEL/IRON PRICE ADJUSTMENT  | DC   | 100      |       |
| 699.040001             | MOBILIZATION   | LS   | 1        |       |

|   |                    |
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| AFFIX SEAL: GREENMAN - PEDERSEN, INC<br>ON:   | ALTERED BY:<br>ON: |
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|  |   |             |                    |          |   |   |
|--|---|-------------|--------------------|----------|---|---|
| AS-BUILT REVISIONS<br>DESCRIPTION OF ALTERATIONS:  | CEMETERY DRIVE OVER THE<br>SOUTH CHUCTANUNDA CREEK BRIDGE REPLACEMENT | PIN 2754.54 | BRIDGES<br>3310200 | CULVERTS | ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED   | CONTRACT NUMBER<br>D036212                      |
|  | TOWN OF FLORIDA   |             |                    |          | ESTIMATE OF QUANTITIES  | DRAWING NO. EQQ-1<br>SHEET NO. 14               |
|  | COUNTY: MONTGOMERY  | REGION: 2   |                    |          |   |   |
| IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION. |   |             |                    |          |  | MONTGOMERY COUNTY<br>DEPARTMENT OF PUBLIC WORKS |

**GENERAL NOTES**

- DESIGN SPECIFICATIONS: NYSDOT LRFD BRIDGE DESIGN SPECIFICATIONS WITH ALL PROVISIONS IN EFFECT AS OF MAY 2022 (FOR DESIGN PURPOSES, COMPRESSIVE STRENGTH OF CONCRETE FOR SUBSTRUCTURES AND DECK SLABS AT 28 DAYS:  $f'c = 3000$  psi.)
- LIVE LOAD: AASHTO HL-93
- CONSTRUCTION AND MATERIALS SPECIFICATIONS: STANDARD SPECIFICATIONS, CONSTRUCTION AND MATERIALS, NEW YORK STATE DEPARTMENT OF TRANSPORTATION, OFFICE OF ENGINEERING.
- DETAILS ON THE DRAWINGS LABELED AS "NOT TO SCALE" ARE INTENTIONALLY DRAWN NOT TO SCALE FOR VISUAL CLARITY. ALL OTHER DETAILS FOR WHICH NO SCALE IS SHOWN ARE DRAWN PROPORTIONAL AND ARE FULLY DIMENSIONED.
- ALL SHOP DRAWINGS SUBMITTED FOR THIS PROJECT SHALL BE IN US CUSTOMARY UNITS.
- THIS BRIDGE SHALL BE MAINTAINED IN ACCORDANCE WITH THE GUIDELINES CONTAINED IN THE CURRENT EDITION OF THE AASHTO MAINTENANCE MANUAL FOR ROADWAYS AND BRIDGES.
- UNLESS OTHERWISE INDICATED ON THE PLANS, WORK TO BE PERFORMED UNDER THIS CONTRACT DOES NOT REQUIRE THE DISTURBING, DESTRUCTION OR REMOVAL OF ANY KNOWN MATERIALS CONTAINING ASBESTOS. UNLESS OTHERWISE INDICATED ON THE PLANS, IT IS THE EXPRESS INTENT OF THIS CONTRACT THAT THESE MATERIALS NOT BE DISTURBED IN ANY WAY. SHOULD THE CONTRACTOR BE FORCED TO DISTURB IN ANY WAY ANY SUCH MATERIALS, THE CONTRACTOR SHALL FIRST BE FAMILIAR WITH INDUSTRIAL CODE RULE 56 OF THE N.Y.S. DEPARTMENT OF LABOR. THE CONTRACTOR SHALL ALSO OBTAIN WRITTEN PERMISSION OF THE E.I.C. BEFORE PROCEEDING.
- THE LOAD RATINGS ARE IN ACCORDANCE WITH THE AASHTO MANUAL FOR BRIDGE EVALUATION.
- ALL DRAWINGS AND CALCULATIONS SUBMITTED BY THE CONTRACTOR FOR ENGINEER REVIEW SHALL BE IN PDF FORMAT AND SHALL BE EITHER 8.5" X 11" OR 11" X 17" WITH TEXT SIZE SHALL BE NO SMALLER THAN 1/16". SUBMISSIONS ILLEGIBLE WHEN PRINTED ON PAPER WILL BE REJECTED. FULL SIZE AND PAPER SUBMISSIONS SHALL NOT BE ACCEPTED OR REVIEWED. ALL DRAWINGS AND CALCULATIONS SHALL NOTE THE DESIGNER AND CHECKER. THE DESIGNER SHALL NOT CHECK THEIR OWN WORK. 11" X 17" DRAWINGS SHALL HAVE A BLANK SPACE THAT IS EITHER 2.5" X 5.5" OR 3.5" X 3.5" FOR PLACEMENT OF THE REVIEWER'S STAMP. 8.5" X 11" DRAWINGS SHALL HAVE AN APPROVAL COVER SHEET IF SPACE FOR THE STAMP DOES NOT FIT ON THE ACTUAL DRAWING.
- DIMENSIONS FOR THICKNESSES OF STEEL ROLLED ANGLE SHAPES AND STRUCTURAL TUBING ARE SHOWN ACCORDING TO THE AISC MANUAL.

**FOUNDATION NOTES**

- HIGHWAY EMBANKMENT MATERIAL (FROM HIGHWAY ESTIMATE OR FROM STRUCTURE EXCAVATION BACKFILL) AND SELECT STRUCTURE FILL, ITEM 203.21, SHALL BE PLACED SIMULTANEOUSLY, IN CONTACT, ON BOTH SIDES OF THE VERTICAL PAYMENT LINE.
- THE COST OF WATER USED FOR COMPACTION OF SELECT FILL ITEMS SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 203.21 - SELECT STRUCTURE FILL.
- ALL PLACEMENTS OF SELECT STRUCTURAL FILL, ITEM 203.21, SHALL BE COMPACTED TO 95 PERCENT OF STANDARD PROCTOR MAXIMUM DENSITY.

**COFFERDAM AND HYDRAULIC NOTES**

- SHOULD THE CONTRACTOR ELECT TO LAY BACK A PORTION OF THE EXISTING EARTH ADJACENT TO AN EXCAVATION REQUIRING A COFFERDAM, ANY REQUIRED EXTENSIONS OF THE COFFERDAM NECESSARY TO KEEP WATER FROM ENTERING THE EXCAVATION SHALL BE FURNISHED AND PLACED AT NO COST TO THE COUNTY.
- WHERE A COFFERDAM IS USED, THE COST OF DEWATERING THE ENTIRE EXCAVATION, REGARDLESS OF THE SOURCE OF WATER, SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE COFFERDAM ITEM.
- THE COFFERDAMS ARE TO BE CONTRACTOR DESIGNED. A COFFERDAM PLAN SHALL BE SUBMITTED TO THE ENGINEER FIFTEEN (15) DAYS PRIOR TO INSTALLATION.
- DEWATERING OF THE COFFERDAM SHALL BE ACCOMPLISHED BY PUMPING THE WATER TO AN APPROVED UPLAND VEGETATED AREA OUTSIDE OF THE STREAMBED AND/OR APPROVED BY THE E.I.C. TEMPORARY SOIL EROSION AND WATER POLLUTION CONTROL MAY BE REQUIRED IN ACCORDANCE WITH DEC BEST PRACTICES. NO SETTLEMENT BASIN SHALL BE CONSTRUCTED.
- ORDINARY HIGH WATER IS ESTIMATED TO BE 581.34 FEET. THIS IS DEFINED AS THE WATER SURFACE ELEVATION FOR THE MEAN ANNUAL FLOOD, WHICH IS THE FLOOD THAT HAS A RECURRENCE INTERVAL OF 2.33 YEARS.
- ORDINARY WATER IS ESTIMATED TO BE 581.00 FEET. ORDINARY WATER IS DEFINED AS THE HIGHEST SURFACE WATER ELEVATION LIKELY TO BE ENCOUNTERED DURING ONE CONSTRUCTION SEASON (EXCLUDING MAJOR FLOODS). IT IS ALWAYS LESS THAN THE ORDINARY HIGH-WATER ELEVATION AND IS USUALLY AN OBSERVED ELEVATION RATHER THAN A COMPUTED ONE.
- LOW WATER IS ESTIMATED TO BE 580.00 FEET. LOW WATER IS DEFINED AS THE NORMAL LOW WATER ELEVATION PREVALENT DURING ONE CONSTRUCTION SEASON FOR MORE THAN 25% OF THE TIME. IT IS AN OBSERVED ELEVATION RATHER THAN A COMPUTED ONE.

**SUPERSTRUCTURE NOTES**

- ALL STRUCTURAL STEEL SHALL CONFORM TO ASTM A709, GRADE 50.
- FOR THE VARIOUS LUMP SUM STRUCTURAL STEEL ITEMS IN THE CONTRACT, THE "TOTAL WEIGHT FOR PROGRESS PAYMENT" IS AS FOLLOWS:

| ITEM     | TOTAL WEIGHT FOR PROGRESS PAYMENT | BIN     |
|----------|-----------------------------------|---------|
| 564.0501 | 49,400 LBS                        | 3310200 |

- THESE WEIGHTS SHALL BE USED IN DETERMINING PARTIAL PAYMENTS AND PROGRESS. UNDER NO CIRCUMSTANCES SHALL THE "TOTAL WEIGHT FOR PROGRESS PAYMENT" BE USED FOR FINAL PAYMENT PURPOSES. THE CONTRACTOR IS ADVISED NOT TO USE THE "TOTAL WEIGHT FOR PROGRESS PAYMENT" AS A BIDDING TOOL. DISCREPANCIES WHICH MAY OCCUR BETWEEN THE TOTAL WEIGHT SHIPPED AND "TOTAL WEIGHT FOR PROGRESS PAYMENT" SHALL NOT BE A BASIS FOR ADDITIONAL COMPENSATION.
- DIAPHRAGMS SHALL BE FABRICATED TO FIT GIRDETS ERECTED WITH THEIR WEBS PLUMB UNDER FULL DEAD LOAD CONDITIONS, ALSO KNOWN AS TOTAL DEAD LOAD FIT (TDLF).
  - ALL STRUCTURAL STEEL FABRICATED UNDER ITEM 564.0501 SHALL BE HOT DIPPED GALVANIZED UNDER ITEM 564.20010008 - HOT-DIP GALVANIZING OF STRUCTURAL STEEL.
  - ALL BOLTS, NUTS, AND WASHERS SHALL BE GALVANIZED IN ACCORDANCE WITH THE NEW YORK STATE STEEL CONSTRUCTION MANUAL.
  - GALVANIZING SHALL BE DONE PRIOR TO DRILLING OF HOLES OR AFTER SUBSIZING. AFTER GALVANIZING, HOLES SHALL BE DRILLED OR REAMED FULL SIZE AS APPROPRIATE.
  - STUD SHEAR CONNECTORS SHALL BE WELDED PRIOR TO GALVANIZING. THE CONTRACTOR'S WORKER SAFETY PLAN SHALL SPECIFY THE TYPE OF WALKING/WORKING SURFACE TO BE USED SO THAT WORKERS DO NOT WALK ON ANY SURFACE WITH INSTALLED SHEAR CONNECTORS.
  - REASONABLE ACCOMMODATIONS FOR THE PREVENTION OF WET STORAGE STAINING (WHITE RUST) OF HOT-DIPPED GALVANIZED (HDG) MATERIALS SHALL BE PROVIDED AT ALL TIMES. STORAGE OF HDG MATERIALS OUTDOORS SHOULD BE AVOIDED IF POSSIBLE. STORAGE (OR SHIPPING) OF HDG MATERIALS IN CONTACT WITH ONE ANOTHER SHALL BE AVOIDED. IF OUTDOOR STORAGE IS UNAVOIDABLE, EXAMPLES OF REASONABLE ACCOMMODATIONS ARE AS FOLLOWS: STORE MATERIALS OFF OF THE GROUND AWAY FROM ALL VEGETATION, USE NON-RESINOUS WOODEN SPACERS TO ALLOW VENTILATION AND AVOID MOISTURE BUILD UP, INCLINE MEMBERS TO ALLOW DRAINAGE. EXAMPLES OF NON-RESINOUS WOOD ARE: POPLAR, ASH AND SPRUCE. WHITE RUST THAT IS DETERMINED TO BE DETRIMENTAL TO THE INTENDED USE OF THE MEMBER OR HAVE A NEGATIVE VISUAL IMPACT ON THE STRUCTURE SHALL BE REPAIRED IN ACCORDANCE WITH THE NYS STEEL CONSTRUCTION MANUAL. WHITE RUST THAT IS DETERMINED TO BE CAUSED BY IMPROPER STORAGE OR SHIPPING OF HDG MATERIALS SHALL BE REPAIRED AT NO COST TO THE COUNTY.
  - STEEL ERECTION NOTES:
    - THE CONTRACTOR SHALL PROVIDE FOR THE STABILITY OF STRUCTURAL STEEL DURING ALL PHASES OF ERECTION AND CONSTRUCTION, AS PROVIDED IN SUBSECTION 204 OF THE NEW YORK STATE STEEL CONSTRUCTION MANUAL (SCM). THE METHODS USED BY THE CONTRACTOR SHALL BE DOCUMENTED ON THE ERECTION DRAWINGS WITH ALL SUPPORTING STABILITY CALCULATIONS SUBMITTED AND STAMPED BY A LICENSED AND REGISTERED NEW YORK STATE PROFESSIONAL ENGINEER AND SUBMITTED TO THE E.I.C. IN ACCORDANCE WITH THE SCM AT LEAST FIFTEEN (15) DAYS PRIOR TO INSTALLATION.
    - THE DESIGN OF THIS STRUCTURE ASSUMES THAT THE STRUCTURAL STEEL IS COMPLETELY ERECTED BEFORE IT IS ALLOWED TO DEFLECT UNDER ITS OWN DEAD LOAD. DEFLECTIONS INCURRED DURING THE VARIOUS STAGES OF THE ERECTION METHOD ARE NOT CONSIDERED. THEREFORE, THE ACTUAL ERECTION METHODS AND SEQUENCES EMPLOYED BY THE CONTRACTOR MAY HAVE A SUBSTANTIAL EFFECT ON THE FINAL STEEL PROFILE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR TAKING ALL NECESSARY COMPENSATORY ACTION TO ENSURE THAT THE FINAL ALIGNMENT AND PROFILE OF THE ERECTED STEEL CONFORMS TO SUBSECTION 1213, 1214, AND 1215 OF THE NEW YORK STATE STEEL CONSTRUCTION MANUAL (SCM). ANY CORRECTIVE WORK NECESSARY TO RE-POSITION PREVIOUSLY ERECTED STEEL TO ACHIEVE ACCEPTABLE ALIGNMENT AND PROFILE MUST BE APPROVED BY THE E.I.C., AND SHALL BE PERFORMED AT NO ADDITIONAL COST TO THE COUNTY.
  - THE PROVISIONS OF THE CURRENT SPECIFICATIONS FOR SUPERSTRUCTURE SLABS ALLOW THE OPTION OF 3 FORMING SYSTEMS FOR THE UNDERSIDE OF THE SLABS. HOWEVER, ON THIS BRIDGE, ONLY THE FOLLOWING OPTION(S) WILL BE PERMITTED: PERMANENT CORRUGATED METAL FORMS OR REMOVABLE WOODEN FORMS.
  - THE CONTRACTOR SHALL MAKE NO DEVIATIONS FROM THE HAUNCH DETAILS SHOWN ON THESE PLANS WITHOUT THE PERMISSION OF THE E.I.C.
  - TOP SURFACES OF NEW BRIDGE DECKS AND APPROACH SLABS SHALL BE SEALED IN ACCORDANCE WITH ITEM 559.01 - PROTECTIVE SEALING OF STRUCTURAL CONCRETE ON NEW BRIDGE DECKS AND DECK OVERLAYS.

**STREAM PROTECTION NOTE**


- DURING THE COURSE OF CONSTRUCTION, THE CONTRACTOR SHALL CONDUCT OPERATIONS IN SUCH A MANNER AS TO PREVENT OR REDUCE TO A MINIMUM ANY DAMAGE TO ANY STREAM FROM POLLUTION BY DEBRIS, SEDIMENT, OR OTHER FOREIGN MATERIAL, OR FROM MANIPULATION OF EQUIPMENT AND/OR MATERIALS IN OR NEAR SUCH STREAMS. THE CONTRACTOR SHALL NOT RETURN DIRECTLY TO A STREAM ANY WATER WHICH HAS BEEN USED FOR WASH PURPOSES OR OTHER SIMILAR OPERATIONS WHICH CAUSE THIS WATER TO BECOME POLLUTED WITH SAND, SILT, CEMENT, OIL, OR OTHER IMPURITIES. IF THE CONTRACTOR USES WATER FROM A STREAM, THE CONTRACTOR SHALL CONSTRUCT AN INTAKE OR TEMPORARY DAM REQUIRED TO PROTECT AND MAINTAIN WATER RIGHTS AND TO SUSTAIN FISH LIFE DOWNSTREAM.
- THE PROJECT PERMIT CONDITIONS DO NOT REQUIRE IN STREAM WORK RESTRICTIONS. HOWEVER, THE CONTRACTOR SHALL FAMILIARIZE THEMSELVES WITH ALL PERMIT REQUIREMENTS. ENVIRONMENTAL PERMITS ARE INCLUDED IN THE PROJECT MANUAL.


**REMOVAL NOTES**

- EXISTING SUBSTRUCTURE SHALL BE REMOVED WITHIN THE LIMITS SHOWN ON THE PLANS UNDER ITEM 202.19.
- EXISTING SUPERSTRUCTURE SHALL BE REMOVED UNDER ITEM 202.120001.
- ACCORDING TO THE REQUIREMENTS OF §202-3.01 GENERAL AND SAFETY REQUIREMENTS, A REMOVAL PLAN SHALL BE SUBMITTED TO THE ENGINEER FIFTEEN (15) DAYS PRIOR TO BEGINNING THE DEMOLITION. THE REQUIREMENT THAT IT BE SIGNED BY A LICENSED AND REGISTERED PROFESSIONAL ENGINEER IS WAIVED.
- RECORD PLANS FOR THIS STRUCTURE ARE NOT AVAILABLE.
- LIMITS AND METHODS FOR THE REMOVAL OF PAINT AT LOCATIONS OF FASTENER REMOVAL OR FLAME CUTTING SHALL MEET THE PROVISIONS OF §202-3.01 - GENERAL, OF THE NYSDOT STANDARD SPECIFICATIONS - CONSTRUCTION AND MATERIALS. THE COST OF PAINT REMOVAL SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE SUPERSTRUCTURE REMOVAL ITEM(S) (OR THE UNIT PRICE BID FOR THE SUBSTRUCTURE REMOVAL ITEM). PAINT WASTE NOT COLLECTED BY VACUUM METHODS SHALL BE COLLECTED USING THE ENVIRONMENTAL GROUND AND/OR WATERWAY PROTECTION ITEM(S). WASTE SHALL BE DISPOSED OF USING THE TREATMENT AND DISPOSAL OF PAINT REMOVAL WASTE ITEM.
- LOOSE AND/OR PEELING PAINT ON STEEL SURFACES MAY BECOME DISLODGED DURING REMOVAL OPERATIONS OR DURING TRANSPORTATION FROM THE SITE UNLESS APPROPRIATE MEASURES ARE TAKEN. THE CONTRACTOR SHALL FORMULATE AND SUBMIT A METHOD OF REMEDIATING THE CONDITION FOR APPROVAL BY THE ENGINEER. WORKER LEAD PROTECTION IN ACCORDANCE WITH 29 CFR 1926.62 SHALL BE SATISFIED. REMEDIATION METHODS COULD INCLUDE TRANSPORTING AFFECTED MEMBERS IN CLOSED TRUCKS, WRAPPING AFFECTED MEMBERS PRIOR TO REMOVAL, ENCAPSULATING THE LOOSE PAINT OR REMOVAL OF LOOSE PAINT PRIOR TO DISMANTLING OPERATIONS. THE COST OF REMEDIATING THIS CONDITION SHALL BE INCLUDED IN THE LUMP SUM PRICE(S) BID FOR THE SUPERSTRUCTURE REMOVAL ITEM(S) (OR THE UNIT PRICE BID FOR THE SUBSTRUCTURE REMOVAL ITEM). THE USE OF ENVIRONMENTAL GROUND AND/OR WATERWAY PROTECTION ITEMS WILL BE REQUIRED, DEPENDING ON THE ALTERNATIVE CHOSEN. THE TREATMENT AND DISPOSAL OF PAINT REMOVAL WASTE ITEM MAY BE REQUIRED. BECAUSE OF THE ABOVE MENTIONED CONDITION, THE CONTRACTOR SHALL EXAMINE THE CONDITION OF THE STRUCTURE'S PAINT PRIOR TO SUBMITTING A BID.

**RECONSTRUCTION NOTES**

- DUE TO THE NATURE OF RECONSTRUCTION PROJECTS, THE EXACT EXTENT OF RECONSTRUCTION WORK CANNOT BE ACCURATELY DETERMINED PRIOR TO THE COMMENCEMENT OF WORK. THESE CONTRACT DOCUMENTS HAVE BEEN PREPARED BASED ON FIELD INSPECTION AND OTHER INFORMATION AVAILABLE AT THIS TIME. ACTUAL FIELD CONDITIONS MAY REQUIRE MODIFICATIONS TO CONSTRUCTION DETAILS AND WORK QUANTITIES. THE CONTRACTOR SHALL PERFORM THE WORK IN ACCORDANCE WITH FIELD CONDITIONS.
- THE CONTRACTOR SHALL PERFORM ALL WORK WITH CARE SO THAT ANY MATERIALS WHICH ARE TO REMAIN IN PLACE, OR WHICH ARE TO REMAIN THE PROPERTY OF THE CITY, WILL NOT BE DAMAGED. IF THE CONTRACTOR DAMAGES ANY MATERIALS WHICH ARE TO REMAIN IN PLACE OR WHICH ARE TO REMAIN THE PROPERTY OF THE COUNTY THE DAMAGED MATERIALS SHALL BE REPAIRED OR REPLACED IN A MANNER SATISFACTORY TO THE ENGINEER AT THE EXPENSE OF THE CONTRACTOR.
- WHENEVER ITEMS IN THE CONTRACT REQUIRE MATERIALS TO BE REMOVED AND DISPOSED OF, THE COST OF SUPPLYING A DISPOSAL AREA AND TRANSPORTATION TO THAT AREA SHALL BE INCLUDED IN THE UNIT PRICES BID FOR THOSE ITEMS.
- DURING REMOVAL OPERATIONS, THE CONTRACTOR SHALL NOT DROP WASTE CONCRETE, DEBRIS, AND OTHER MATERIAL TO THE AREA BELOW THE BRIDGE EXCEPT WHERE THE PLANS SPECIFICALLY PERMIT THE DROPPING OF MATERIAL. PLATFORMS, NETS, SCREENS OR OTHER PROTECTIVE DEVICES SHALL BE USED TO CATCH THE MATERIAL. IF ADEQUATE PROTECTIVE DEVICES ARE NOT BEING EMPLOYED, THE WORK WILL BE STOPPED UNTIL ADEQUATE PROTECTION IS PROVIDED.
- ALL MATERIAL FALLING ON THE AREA BELOW AND ADJACENT TO THE BRIDGE SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AT NO COST TO THE COUNTY.
- THE COST OF FURNISHING, INSTALLING, MAINTAINING, REMOVING AND DISPOSING OF ALL PLATFORMS, NETS, SCREENS OR OTHER PROTECTIVE DEVICES SHALL BE INCLUDED IN THE PRICES BID FOR THE APPROPRIATE ITEMS OF THE CONTRACT.
- THE CONTRACTOR SHALL MOUNT A NEW BRIDGE IDENTIFICATION NUMBER 3310200 PLATE THAT MEETS THE FOLLOWING MATERIAL REQUIREMENTS:  
 PANEL WITH REFLECTIVE BACKGROUND: THE ALUMINUM PANEL SHALL CONFORM TO THE REQUIREMENTS OF THE NYSDOT STANDARD SPECIFICATIONS. THE BACKGROUND MATERIAL SHALL BE GREEN REFLECTIVE SHEETING CONFORMING TO THE REQUIREMENTS OF THE NYSDOT STANDARD SPECIFICATION FOR CLASS A SHEETING. THE SIZE OF THE PANELS SHALL BE 1/8" INCH THICK BY 3 INCHES BY 12 INCHES. A THIN RUBBER OR PLASTIC GASKET OR SHEETING MATCHING THE PLATE SIZE SHALL BE PLACED BEHIND THE PLATE PRIOR TO INSTALLATION.  
 NUMBERS: THE NUMBERS SHALL BE REFLECTIVE SHEETING CONFORMING TO THE REQUIREMENTS OF THE NYSDOT STANDARD SPECIFICATION FOR CLASS A SHEETING, EXCEPT THAT THE ADHESIVE SHALL BE PRESSURE-SENSITIVE SUCH THAT THE NUMBERS CAN BE APPLIED TO THE BACKGROUND IN THE FIELD. THE NUMBERS SHALL BE 2 INCHES HIGH AND SILVER-WHITE IN COLOR CONFORMING TO FHWA SERIES C DIMENSIONS.  
 PRIOR TO PLACING THE NUMBERS ON THE PANEL, THE REFLECTIVE BACKGROUND SHALL BE CLEAN AND FREE OF DIRT AND OIL WHICH MAY ADVERSELY AFFECT PROPER ADHESION. THE NUMBERS SHALL BE PLACED ON THE REFLECTIVE BACKGROUND, PERPENDICULAR TO THE LONGITUDINAL AXIS OF THE PANEL AND VERTICALLY CENTERED. THE REFLECTIVE BACKGROUND AND NUMBERS SHALL BE COATED AND/OR EDGE SEALED IN ACCORDANCE WITH THE RECOMMENDATIONS OF THE SHEETING MANUFACTURER.  
 THE BIN PLATE SHALL BE ATTACHED TO THE BEGINNING ABUTMENT, RIGHT SIDE OF THE BRIDGE USING EXPANSION ANCHORS. THE PLATE SHALL BE PLACED HIGH ON THE ABUTMENT, NEAR THE FASCIA OF THE BRIDGE SO THAT IT CANNOT BE PAINTED OVER VIA A SPRAY PAINT CAN OR EASILY REMOVED OR DAMAGED.  
 THE COST ASSOCIATED WITH PROVIDING AND INSTALLING THE NEW BIN PLATE WILL BE INCLUDED IN THE PRICE BID FOR VARIOUS CONTRACT ITEMS.

|   |                    |
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| AFFIX SEAL: GREENMAN - PEDERSEN, INC<br>ON:   | ALTERED BY:<br>ON: |
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|  |  |             |                    |          |   |                            |                                   |
|--|--|-------------|--------------------|----------|---|----------------------------|-----------------------------------|
| AS-BUILT REVISIONS<br>DESCRIPTION OF ALTERATIONS:  | CEMETERY DRIVE OVER THE                    | PIN 2754.54 | BRIDGES<br>3310200 | CULVERTS | ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED     | CONTRACT NUMBER<br>D036212 |                                   |
|  | SOUTH CHUCTANUNDA CREEK BRIDGE REPLACEMENT |             |                    |          |   |                            |                                   |
|  | TOWN OF FLORIDA                            |             |                    |          |   |                            |                                   |
|  | COUNTY: MONTGOMERY REGION: 2               |             |                    |          |   |                            |                                   |
| IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION. |  |             |                    |          |   | GENERAL NOTES<br>(1 OF 2)  | DRAWING NO. GNN-1<br>SHEET NO. 15 |
|   |  |             |                    |          | MONTGOMERY COUNTY<br>DEPARTMENT OF PUBLIC WORKS |                            |                                   |

**HIGHWAY GENERAL NOTES**

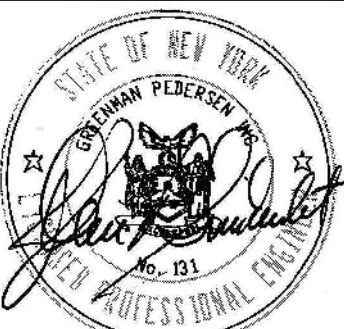
1. CURRENT NATIONAL "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES" (MUTCD) WITH NEW YORK STATE SUPPLEMENT SHALL BE IN EFFECT FOR THIS PROJECT
2. ADDITIONAL NOTES MAY BE FOUND ON SUBSEQUENT DRAWINGS. SUCH NOTES, WHILE PERTAINING TO THE SPECIFIC DRAWING THEY ARE PLACED ON, ALSO SUPPLEMENT THE GENERAL NOTES LISTED HEREIN.
3. THE CONTRACTOR SHALL EXAMINE AND VERIFY IN THE FIELD ALL EXISTING CONDITIONS AND DIMENSIONS WITH THOSE SHOWN ON THE PLANS. THE CONTRACTOR SHALL USE THE FIELD CONDITIONS AND DIMENSIONS, AND MAKE THE APPROPRIATE CHANGES TO THOSE SHOWN ON THE PLANS AS APPROVED BY THE ENGINEER. THE RESULTS OF THIS CHECK OF CONDITIONS AND DIMENSIONS SHALL BE SO NOTED ON THE DRAWINGS SUBMITTED FOR APPROVAL.
4. THERE SHALL BE NO CLAIM AGAINST THE COUNTY BY THE CONTRACTOR FOR WORK PERTAINING TO MODIFICATIONS AS MAY BE REQUIRED DUE TO ANY DIFFERENCE BETWEEN ACTUAL FIELD CONDITIONS AND THOSE SHOWN BY THE DETAILS AND DIMENSIONS ON THE CONTRACT PLANS. THE CONTRACTOR WILL BE PAID AT THE UNIT BID PRICE FOR THE ACTUAL QUANTITIES OF MATERIALS USED OR FOR THE WORK PERFORMED, AS INDICATED BY THE VARIOUS ITEMS IN THE CONTRACT.
5. AT ALL TIMES, THE CONTRACTOR SHALL TAKE MEASURES TO PROVIDE POSITIVE DRAINAGE OF SURFACE RUNOFF FROM THE TRAVEL LANES AND CONTROL OF THE RUNOFF TO PREVENT EROSION, POLLUTION, SEDIMENTATION OR OTHER DISCHARGES WHICH WOULD AFFECT PROPERTIES ADJACENT TO THE WORK SITE. ALL MEASURES TAKEN TO PROVIDE POSITIVE DRAINAGE SHALL BE APPROVED BY THE ENGINEER PRIOR TO IMPLEMENTATION. THE COST FOR THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR VARIOUS ITEMS IN THE CONTRACT.
6. THE CONTRACTOR SHOULD NOTE THAT ADDITIONAL WORK MAY BE REQUIRED AS THE CONTRACT PROGRESSES WHICH IS NOT SHOWN OR NOTED ON THE PLANS. THIS WORK SHALL BE PERFORMED BY THE CONTRACTOR AS ORDERED BY THE ENGINEER AND PAYMENT SHALL BE MADE AT THE BID PRICE FOR THE APPROPRIATE ITEMS.
7. NO PAYMENT SHALL BE MADE FOR WORK CALLED FOR BY NOTES ON THE PLANS, IN THE SPECIFICATIONS, OR UNDER THE HEADING GENERAL NOTES UNLESS PAYMENT IS SPECIFICALLY INDICATED BY ITEM NUMBER. THE COST OF WORK FOR WHICH NO PAYMENT IS INDICATED SHALL BE INCLUDED IN THE UNIT PRICES BID FOR THE VARIOUS ITEMS IN THE CONTRACT.
8. THE CONTRACTOR SHALL PROVIDE ALL TEMPORARY SUPPORTS, BRACING OR OTHER DEVICES THAT MAY BE REQUIRED OR THAT MAY BE DIRECTED BY THE ENGINEER TO PROTECT THE SAFETY OF ADJACENT STRUCTURES, ROADWAYS OR THE VARIOUS ITEMS IN THE CONTRACT. NO SEPARATE PAYMENT SHALL BE MADE.
9. PAVED AREAS DISTURBED BY THE CONTRACTOR WHICH ARE NOT PART OF THE WORK TO BE PERFORMED UNDER THIS CONTRACT, SHALL BE RESTORED TO AN ACCEPTABLE CONDITION AS SPECIFIED BY AND SATISFACTORY TO THE ENGINEER. NO PAYMENT WILL BE MADE FOR RESTORING AREAS NOT PART OF THIS CONTRACT.
10. PROVISIONS TO DE-WATER EXCAVATIONS, DUE TO CONSTRUCTION OPERATIONS ALONG THE PROJECT MAY BE REQUIRED, THERE SHALL BE NO DIRECT PAYMENT FOR ANY DE-WATERING SYSTEMS. COST SHALL BE INCLUDED IN THE PRICE BID FOR VARIOUS ITEMS IN THE CONTRACT.
11. THE CONTRACTOR SHALL KEEP ALL DRAINAGE FACILITIES, WITHIN THE CONTRACT LIMITS, CLEAN AND FULLY OPERATIONAL AT ALL TIMES (A.O.B.E.). THIS WORK SHALL BE INCLUDED UNDER VARIOUS ITEMS IN THE CONTRACT.
12. THE CONTRACTOR SHALL PROVIDE SURVEY AND STAKEOUT AS REQUIRED AND IN ACCORDANCE WITH SECTION 625 OF THE STANDARD SPECIFICATIONS. COST FOR THIS WORK SHALL BE INCLUDED UNDER ITEM 625.01-SURVEY OPERATIONS.
13. THE CONTRACTOR IS TO VISIT THE SITE BEFORE BIDDING TO BECOME FAMILIAR WITH THE PRESENT CONDITIONS AND TO JUDGE THE EXTENT AND NATURE OF THE WORK TO BE DONE UNDER THIS CONTRACT. NO EXTRA COMPENSATION WILL BE ALLOWED BECAUSE OF FAILURE TO INCLUDE IN THE BID ALL ITEMS AND MATERIALS WHICH ARE REQUIRED TO BE FURNISHED IN ACCORDANCE WITH THE CONTRACT DOCUMENTS.
14. THE CONTRACTOR SHALL BE REQUIRED TO PROTECT HIS WORKERS AT ALL TIMES IN CONFORMANCE WITH APPLICABLE OSHA REGULATIONS.

**RIGHT-OF-WAY NOTES**

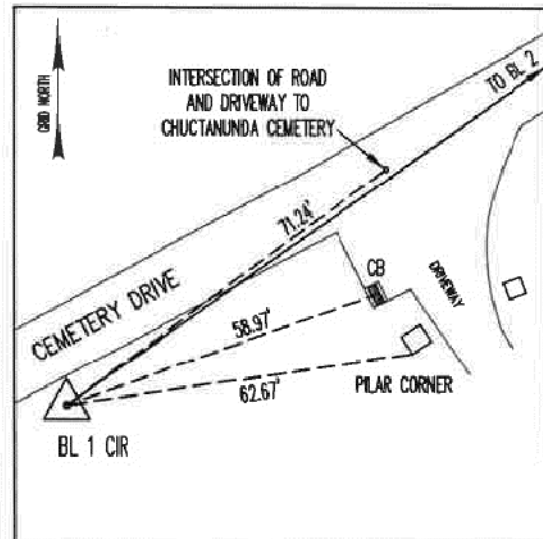
1. FOR THIS PROJECT THE PUBLIC RIGHT-OF-WAY, AS REFERENCED IN THE SUBSEQUENT RIGHT-OF-WAY NOTES, SHALL INCLUDE THE TEMPORARY EASEMENTS, PERMANENT EASEMENTS AND PUBLIC RIGHT-OF-WAY AS SHOWN ON THE PLANS HEREIN.
2. ALL WORK TO BE PERFORMED UNDER THIS CONTRACT WILL BE WITHIN THE PUBLIC RIGHT-OF-WAY (ROW) IN ACCORDANCE WITH SECTION 105-15 OF THE STANDARD SPECIFICATIONS. THE CONTRACTOR IS TO ASSURE HIMSELF THAT ALL WORK IS PERFORMED WITHIN THE ROW, INCLUDING BUT NOT LIMITED TO VEHICLE ACCESS; STORAGE OF EQUIPMENT, MATERIALS DEBRIS AND WASTE; LANDSCAPING; VEGETATION REMOVAL AND MANAGEMENT; GRADING, SEEDING AND THE INSTALLATION OF TURF; AND THE INSTALLATION OF ANY FENCES OR PROTECTIVE BARRIER.
3. IF THE CONTRACTOR IS UNABLE TO IDENTIFY THE LIMITS OF THE RIGHTS-OF-WAY WHEN THE CONTRACT CALLS FOR WORK IN THOSE VICINITIES, THE CONTRACTOR MUST CONTACT THE PROJECT ENGINEER FOR DEFINITIVE BOUNDARY DETERMINATIONS BEFORE ANY WORK MAY BE INITIATED AT THOSE LOCATIONS (STANDARD SPECIFICATION SECTIONS 105-10 AND 625).
4. IN ACCORDANCE WITH SECTION 105-15 OF THE STANDARD SPECIFICATIONS, RELEASES FOR ANY NON-ESSENTIAL CONTRACT WORK OUTSIDE OF THE EXISTING RIGHTS-OF-WAY, INCLUDING PLANTINGS, LANDSCAPING OR DRIVEWAY ENHANCEMENT, WILL BE PROVIDED BY THE PROJECT ENGINEER AND IN NO INSTANCE ARE TO BE SECURED BY THE CONTRACTOR. THE CONTRACTOR SHALL NOT INVADE UPON PRIVATE PROPERTIES, LANDS OR BUILDINGS OUTSIDE OF THE RIGHTS-OF-WAY FOR ANY REASON WITHOUT FIRST SECURING WRITTEN PERMISSION FROM THE PROPERTY OWNER (STANDARD SPECIFICATIONS SECTION 105-15).
5. THE CONTRACTOR WILL BE HELD LIABLE FOR ANY DAMAGES DONE. ANY SUCH INJURIES OR DAMAGES SHALL BE SATISFACTORILY REPAIRED OR ITEMS REPLACED AT THE CONTRACTOR'S EXPENSE (STANDARD SPECIFICATIONS SECTION 107-08).

**UTILITY NOTES**

1. NO UNDERGROUND UTILITIES ARE KNOWN TO EXIST WITHIN THE PROJECT LIMITS. A STORM WATER DRAINAGE PIPE IS LOCATED BENEATH THE FARM ACCESS ROAD AS SHOWN ON THE PLANS HEREIN.
2. OVERHEAD ELECTRICAL AND COMMUNICATION LINES ARE LOCATED WITHIN THE PROJECT LIMITS. NO RELOCATION OF OVERHEAD UTILITIES IS SCHEDULED OR ANTICIPATED.
3. EXACT LOCATIONS OF UTILITIES, PUBLIC AND/OR PRIVATE, SHALL BE DETERMINED IN THE FIELD BY THE CONTRACTOR. PRIOR TO ANY EXCAVATION, THE CONTRACTOR IS TO CALL DIG SAFELY N.Y. TO HAVE UNDERGROUND UTILITIES LOCATED.
4. IN THE EVENT THE CONTRACTOR DAMAGES AN EXISTING UTILITY SERVICE, CAUSING THE INTERRUPTION IN SAID SERVICE, THE CONTRACTOR SHALL IMMEDIATELY COMMENCE WORK TO RESTORE SERVICE AND MAY NOT CEASE WORK UNTIL SERVICE IS RESTORED. ALL COSTS TO REPAIR OR REPLACE DAMAGED UTILITIES SHALL BE AT THE EXPENSE OF THE CONTRACTOR. IF THE CONTRACTOR DOES NOT MAKE IMMEDIATE NECESSARY REPAIRS, THE RESPECTIVE OWNING COMPANIES OR MUNICIPAL FORCES MAY DO THE WORK, AND THE COST THEREOF CHARGED TO THE CONTRACTOR.
5. THE CONTRACTOR SHALL PROTECT ALL UNDERGROUND UTILITIES TO REMAIN IN PLACE FROM DAMAGE DURING THE CONSTRUCTION. METHODS OF PROTECTION MAY INCLUDE STEEL PLATES OVER THE UTILITY SO THAT WHEEL LOADING FROM CONSTRUCTION VEHICLES DO NOT DAMAGE THE UTILITY. THE COST OF PROVIDING PROTECTION OF UNDERGROUND UTILITIES SHALL BE INCLUDED UNDER VARIOUS ITEMS IN THE CONTRACT.

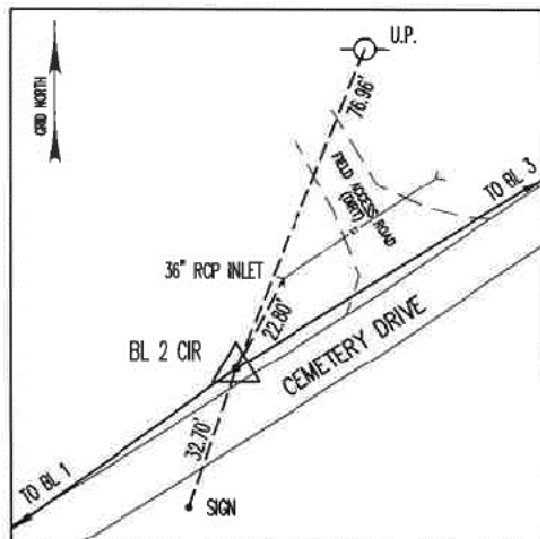
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| AFFIX SEAL: GREENMAN - PEDERSEN, INC<br>ON:   | ALTERED BY:<br>ON: |
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|--|---|-------------|--------------------|----------|--|---|
| AS-BUILT REVISIONS<br>DESCRIPTION OF ALTERATIONS:  | CEMETERY DRIVE OVER THE<br>SOUTH CHUCTANUNDA CREEK BRIDGE REPLACEMENT | PIN 2754.54 | BRIDGES<br>3310200 | CULVERTS | ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED                | CONTRACT NUMBER<br>D036212                      |
|  | TOWN OF FLORIDA   |             |                    |          | GENERAL NOTES<br>(2 OF 2)                                  | DRAWING NO. GNN-2<br>SHEET NO. 16               |
| IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION. |   |             |                    |          | <b>GPI</b> GREENMAN-PEDERSEN, INC.<br>CONSULTING ENGINEERS | MONTGOMERY COUNTY<br>DEPARTMENT OF PUBLIC WORKS |



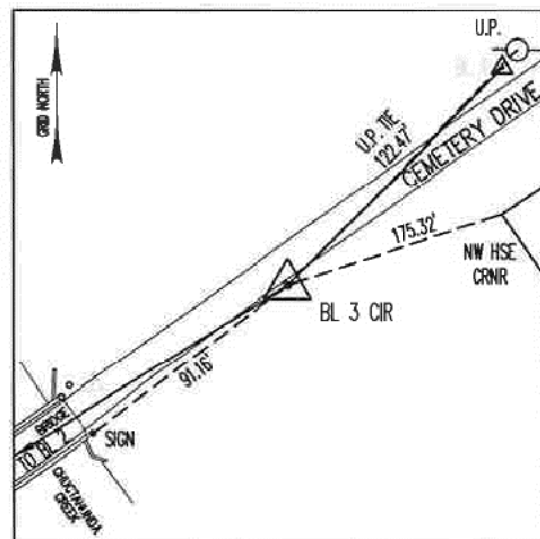
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| BL 1 STA. 10+00.00 | CIR           |
| N: 1473499.901     | E: 569135.657 |
| NOT TO SCALE       |               |

NYSPCS: NAD83 ZONE: EAST  
 U.S. SURVEY FOOT  
 BL 1 IS A CIR LOCATED ON THE SOUTH SIDE OF CEMETERY DRIVE 4.62'± FROM THE SOUTH EDGE OF PAVEMENT AND BEING 813'± WEST OF N.Y.S. ROUTE 30 INTERSECTION



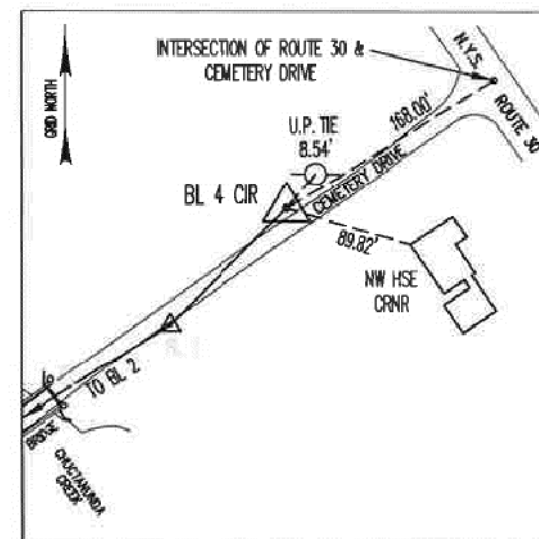
|                    |               |
|--------------------|---------------|
| BL 2 STA. 12+43.05 | CIR           |
| N: 1473640.014     | E: 569334.253 |
| NOT TO SCALE       |               |

NYSPCS: NAD83 ZONE: EAST  
 U.S. SURVEY FOOT  
 BL 2 IS A CIR LOCATED ON THE NORTH SIDE OF CEMETERY DRIVE 3.06'± FROM THE NORTH EDGE OF PAVEMENT AND BEING 570'± WEST OF N.Y.S. ROUTE 30 INTERSECTION



|                    |               |
|--------------------|---------------|
| BL 3 STA. 15+33.55 | CIR           |
| N: 1473793.733     | E: 569580.751 |
| NOT TO SCALE       |               |

NYSPCS: NAD83 ZONE: EAST  
 U.S. SURVEY FOOT  
 BL 3 IS A CIR LOCATED ON THE SOUTH SIDE OF CEMETERY DRIVE 1.97'± FROM THE SOUTH EDGE OF PAVEMENT AND BEING 280'± WEST OF N.Y.S. ROUTE 30 INTERSECTION



|                    |               |
|--------------------|---------------|
| BL 4 STA. 16+47.50 | CIR           |
| N: 1473874.850     | E: 569660.789 |
| NOT TO SCALE       |               |

NYSPCS: NAD83 ZONE: EAST  
 U.S. SURVEY FOOT  
 BL 4 IS A CIR LOCATED ON THE NORTH SIDE OF CEMETERY DRIVE 5.97'± FROM THE NORTH EDGE OF PAVEMENT AND BEING 168'± WEST OF N.Y.S. ROUTE 30 INTERSECTION

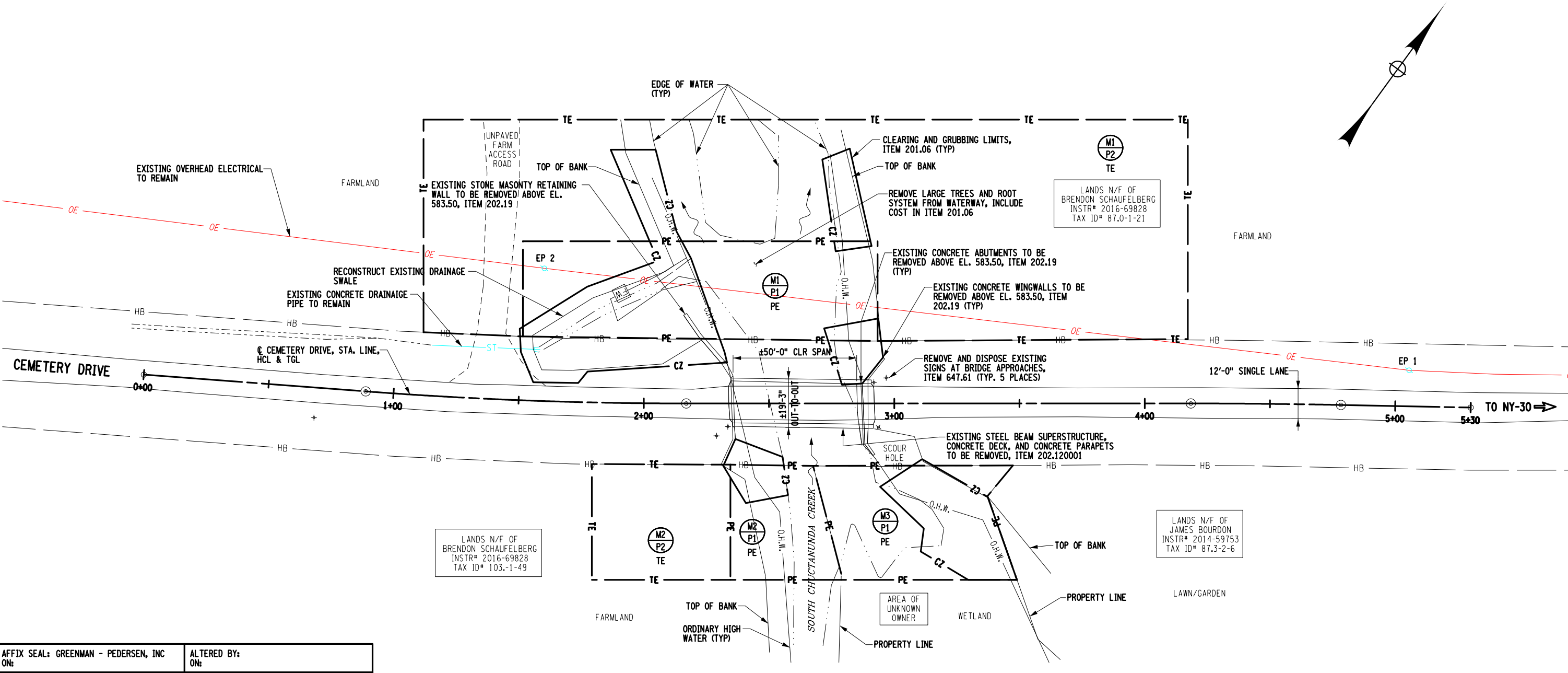
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| AFFIX SEAL: GREENMAN - PEDERSEN, INC<br>ON: | ALTERED BY:<br>ON: |
|   |                    |

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|--|--|-------------|-----------------|----------|---|---|---|
| AS-BUILT REVISIONS<br>DESCRIPTION OF ALTERATIONS:  | CEMETERY DRIVE OVER THE                    | PIN 2754.54 | BRIDGES 3310200 | CULVERTS | ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED | CONTRACT NUMBER<br>D036212                      |   |
|  | SOUTH CHUCTANUNDA CREEK BRIDGE REPLACEMENT |             |                 |          |   | BASELINE TIES                                   | DRAWING NO. BT-1<br>SHEET NO. 17                |
|  | TOWN OF FLORIDA                            |             |                 |          |   |   |   |
|  | COUNTY: MONTGOMERY                         | REGION: 2   |                 |          |   |   |   |
| IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION. |  |             |                 |          |   | GREENMAN-PEDERSEN, INC.<br>CONSULTING ENGINEERS | MONTGOMERY COUNTY<br>DEPARTMENT OF PUBLIC WORKS |

PROJECT MANAGER: S. KERN  
 CHECK: S. KERN  
 DRAFTING: F. CUOCCIO  
 CHECK: B. WEAVER  
 DESIGN: S. KERN  
 JOB MANAGER: D. THOMAS  
 DESIGN SUPERVISOR: J. SIMKULET

| TABLE OF TEMPORARY AND PERMANENT RIGHT OF WAY ACQUISITIONS |            |                      |             |             |                           |       |                                |
|--|------------|----------------------|-------------|-------------|---------------------------|-------|--------------------------------|
| MAP NO.  | PARCEL NO. | REPUTED OWNER        | TAX MAP NO. | ACQUS. TYPE | PARCEL SIZE (+/-) SQ. FT. | ACRES | ACQUISITION SIZE SQ. FT. ACRES |
| 1  | 1          | BRENDON SCHAUFELBERG | 87.0-1-21   | PE          | 5,832,684                 | 133.9 | 5,503 0.13                     |
|  | 2          | SCHAUFELBERG         |             | TE          |                           |       | 21,167 0.49                    |
| 2  | 1          | BRENDON SCHAUFELBERG | 103.-1-49   | PE          | 518,364                   | 11.9  | 1,778 0.04                     |
|  | 2          | SCHAUFELBERG         |             | TE          |                           |       | 2,551 0.06                     |
| 3  | 1          | UNKNOWN OWNER        | N/A         | PE          | N/A                       | N/A   | 3,150 0.07                     |

| OVERHEAD UTILITY DISPOSITION TABLE                          |             |                            |                         |     |       |      |       |  |                                |     |            |     |                         |
|---|-------------|----------------------------|-------------------------|-----|-------|------|-------|--|--------------------------------|-----|------------|-----|-------------------------|
| PIN 2754.54 PROJECT NAME: CEMETERY DRIVE BRIDGE REPLACEMENT |             |                            |                         |     |       |      |       |  |                                |     |            |     |                         |
| EP POLE NUMBER ON PLANS                                     | PAGE NUMBER | UTILITIES REAL POLE NUMBER | FACILITIES ON STRUCTURE |     |       |      |       | PROPOSED DESIGN CONFLICT WITH EXISTING UTILITIES | RELOCATION OR REMOVAL REQUIRED |     | WITHIN ROW |     | UNDERGROUND POWER RISER |
|   |             |                            | DIST                    | SEC | LIGHT | FIRE | CABLE |  | TEL                            | YES | NO         | YES | NO                      |
| EP 1  | ST-1        | NONE                       | X                       | X   |       |      | X     | X  | NONE                           |     | X          | X   |                         |
| EP 2  | ST-1        | NONE                       | X                       | X   |       |      | X     | X  | NONE                           |     | X          | X   |                         |



**GENERAL REMOVAL PLAN**



AFFIX SEAL: GREENMAN - PEDERSEN, INC  
 ON:

ALTERED BY:  
 ON:

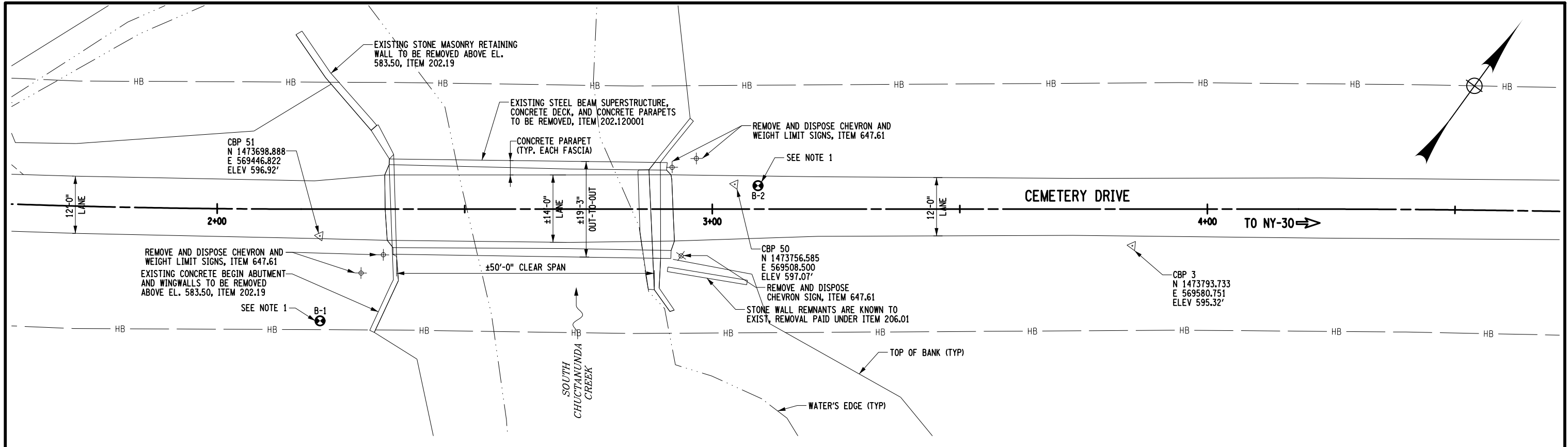
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| AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:   | CEMETERY DRIVE OVER THE SOUTH CHUCTANUNDA CREEK BRIDGE REPLACEMENT | PIN 2754.54 | BRIDGES 3310200 | CULVERTS | ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED             | CONTRACT NUMBER D036212                      |
|  | TOWN OF FLORIDA  |             |                 |          | GENERAL REMOVAL PLAN                                    | DRAWING NO. ST-1                             |
|  | COUNTY: MONTGOMERY   | REGION: 2   |                 |          |   | SHEET NO. 18                                 |
| IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION. |  |             |                 |          | <b>GPI</b> GREENMAN-PEDERSEN, INC. CONSULTING ENGINEERS | MONTGOMERY COUNTY DEPARTMENT OF PUBLIC WORKS |

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 USER = skern

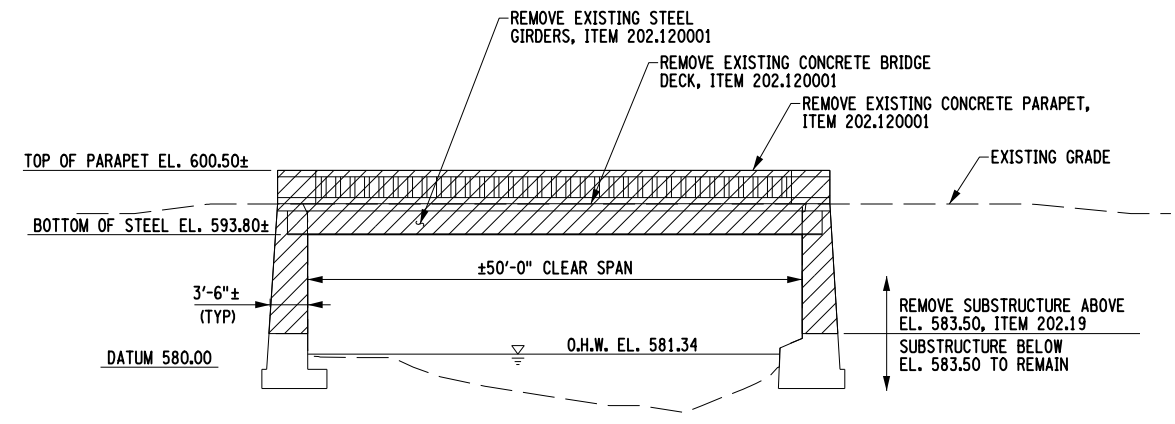


PROJECT MANAGER S. KERN  
 CHECK S. KERN  
 DRAFTING F. CIUCCIO  
 CHECK B. WEAVER  
 DESIGN S. KERN  
 JOB MANAGER D. THOMAS  
 DESIGN SUPERVISOR J. SIMKULET

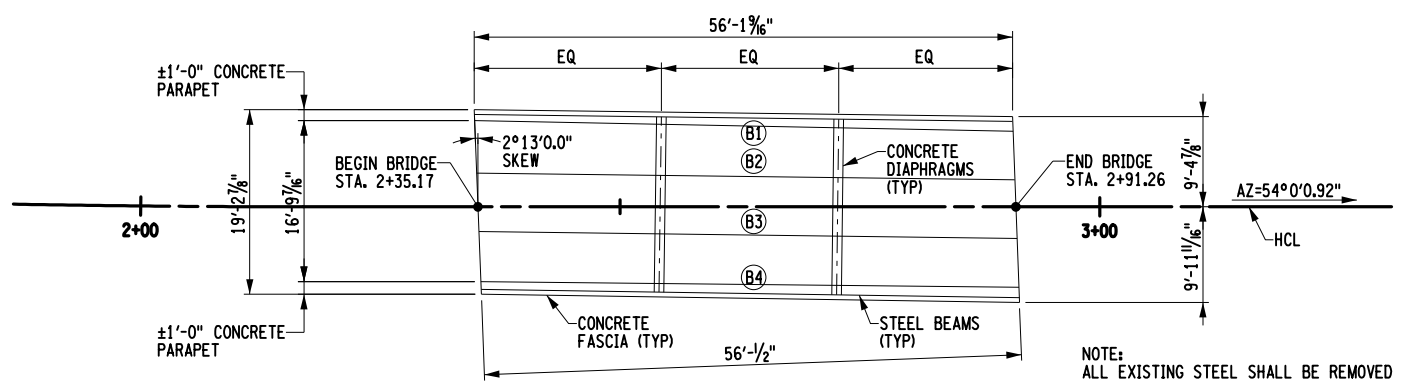
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PLAN



ELEVATION



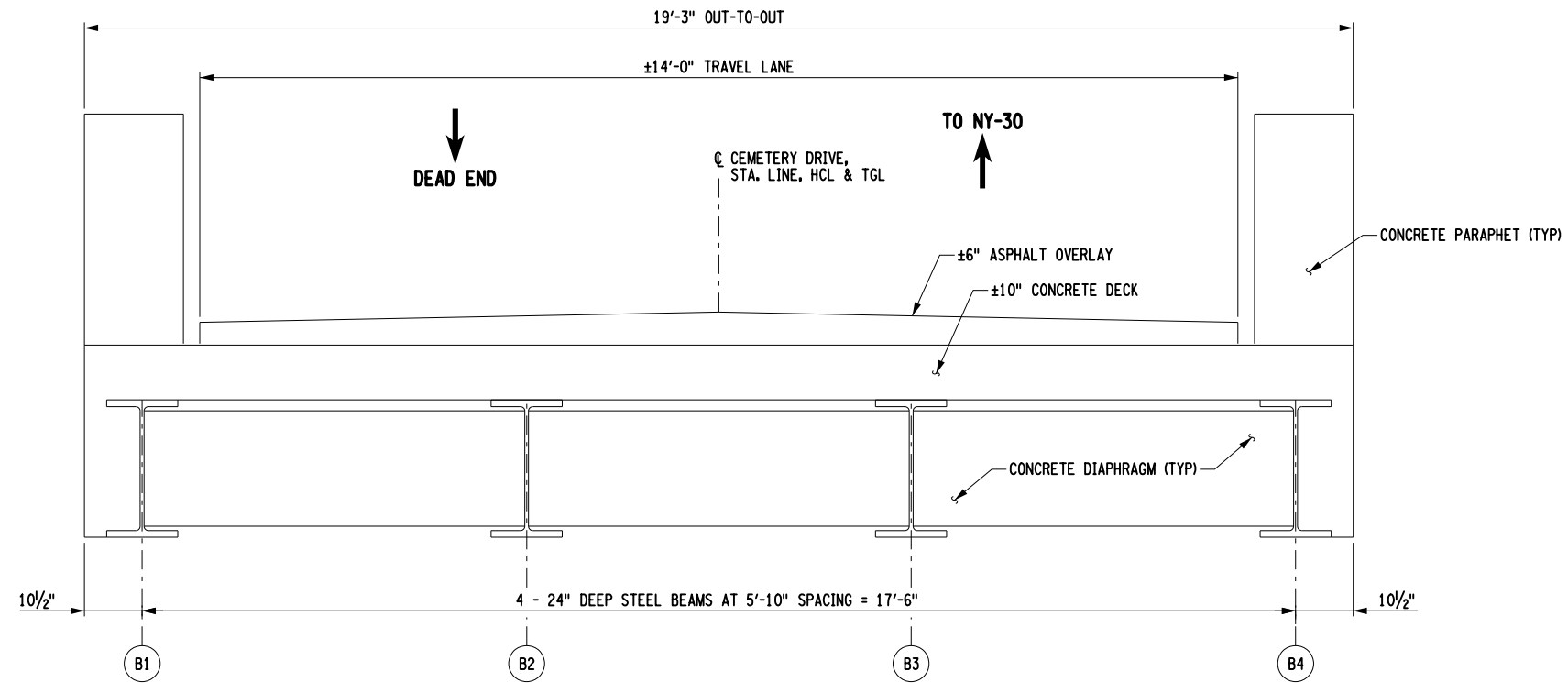
EXISTING FRAMING PLAN

- NOTES:**
- FOR BORING DATA, SEE THE SUBSURFACE EXPLORATION DATA REPORT INCLUDED AS SUPPLEMENTAL INFORMATION IN THE PROJECT MANUAL.
  - THE EXISTING STRUCTURE SHOWN IS FOR ESTIMATING PURPOSES ONLY AND MAY NOT REFLECT THE ACTUAL EXISTING CONDITIONS.

AFFIX SEAL: GREENMAN - PEDERSEN, INC  
 ON: \_\_\_\_\_  
 ALTERED BY:  
 ON: \_\_\_\_\_



|  |  |             |                 |          |   |  |  |
|--|--|-------------|-----------------|----------|---|--|--|
| AS-BUILT REVISIONS<br>DESCRIPTION OF ALTERATIONS:  | CEMETERY DRIVE OVER THE                    | PIN 2754.54 | BRIDGES 3310200 | CULVERTS | ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED | CONTRACT NUMBER D036212                          |  |
|  | SOUTH CHUCTANUNDA CREEK BRIDGE REPLACEMENT |             |                 |          | REMOVAL PLAN                                | DRAWING NO. ST-2                                 |  |
|  | TOWN OF FLORIDA                            |             |                 |          |   |  | SHEET NO. 19                                 |
|  | COUNTY: MONTGOMERY                         | REGION: 2   |                 |          |   |  |  |
| IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION. |  |             |                 |          |   | GPI GREENMAN-PEDERSEN, INC. CONSULTING ENGINEERS | MONTGOMERY COUNTY DEPARTMENT OF PUBLIC WORKS |



**EXISTING TYPICAL SECTION**  
 SCALE: 3/8"=1'-0"

**NOTES:**

1. THE EXISTING STRUCTURE SHOWN IS FOR ESTIMATING PURPOSES ONLY AND MAY NOT REFLECT THE ACTUAL EXISTING CONDITIONS.
2. ALL SUPERSTRUCTURE ELEMENTS SHOWN ARE TO BE REMOVED AND PAID UNDER ITEM 202.120001.

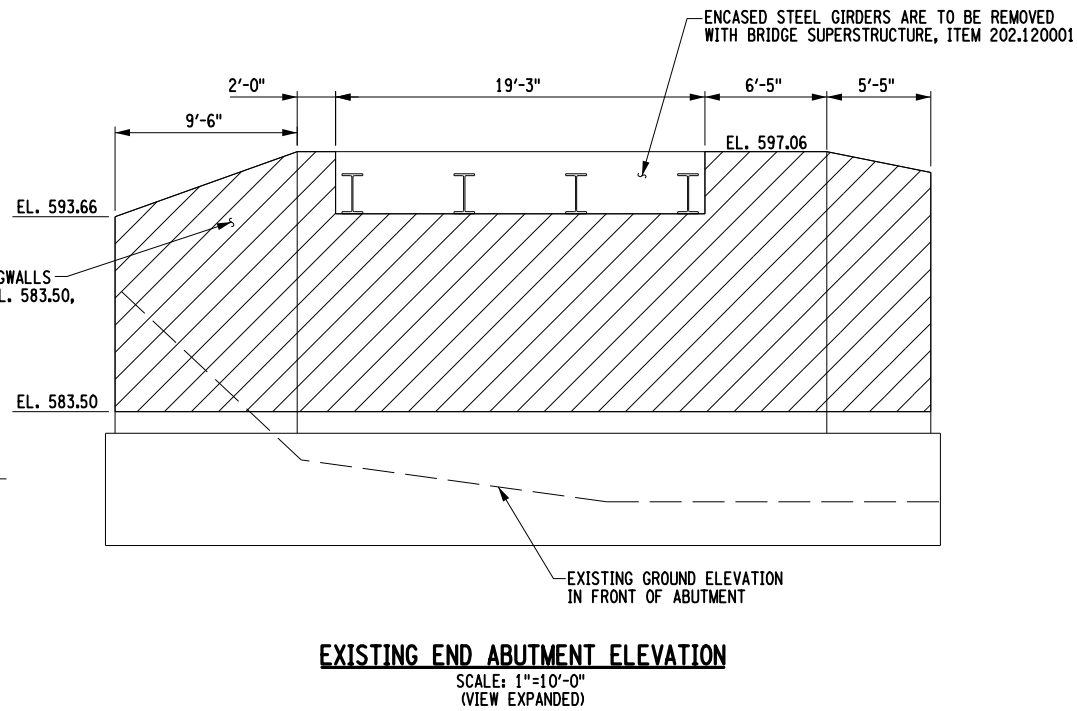
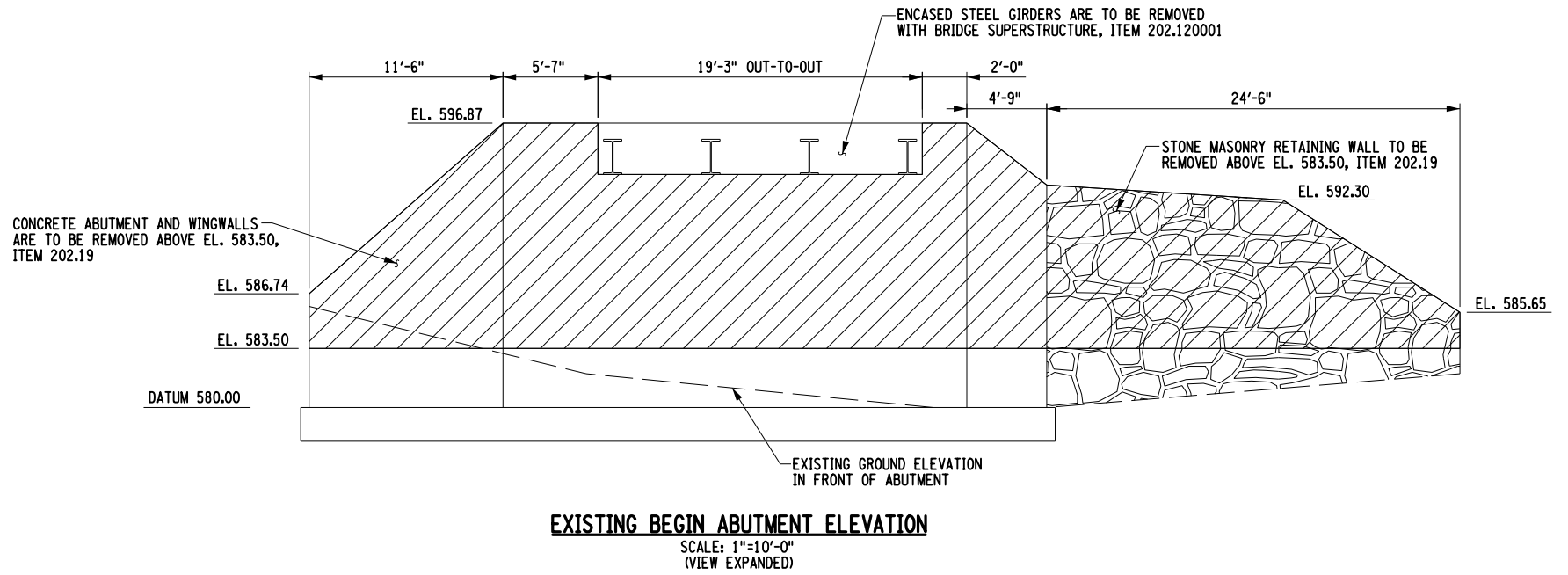
AFFIX SEAL: GREENMAN - PEDERSEN, INC  
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| AS-BUILT REVISIONS<br>DESCRIPTION OF ALTERATIONS:  | CEMETERY DRIVE OVER THE                    | PIN 2754.54 | BRIDGES<br>3310200 | CULVERTS | ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED | CONTRACT NUMBER<br>D036212                                 |
|  | SOUTH CHUCTANUNDA CREEK BRIDGE REPLACEMENT |             |                    |          |   | DRAWING NO. ST-3<br>SHEET NO. 20                           |
|  | TOWN OF FLORIDA                            |             |                    |          | EXISTING TYPICAL SECTION                    |  |
|  | COUNTY: MONTGOMERY                         | REGION: 2   |                    |          |   | MONTGOMERY COUNTY<br>DEPARTMENT OF PUBLIC WORKS            |
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 DATE/TIME = 4/13/2022  
 USER = skern

PROJECT MANAGER S. KERN  
 CHECK S. KERN  
 DRAFTING F. CIUCCIO  
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 DESIGN S. KERN  
 JOB MANAGER D. THOMAS  
 DESIGN SUPERVISOR J. SIMKULET



- NOTES:**
1. THE EXISTING STRUCTURE, DIMENSIONS, AND ELEVATIONS SHOWN ARE FOR ESTIMATING PURPOSES ONLY AND MAY NOT REFLECT THE ACTUAL EXISTING CONDITIONS.

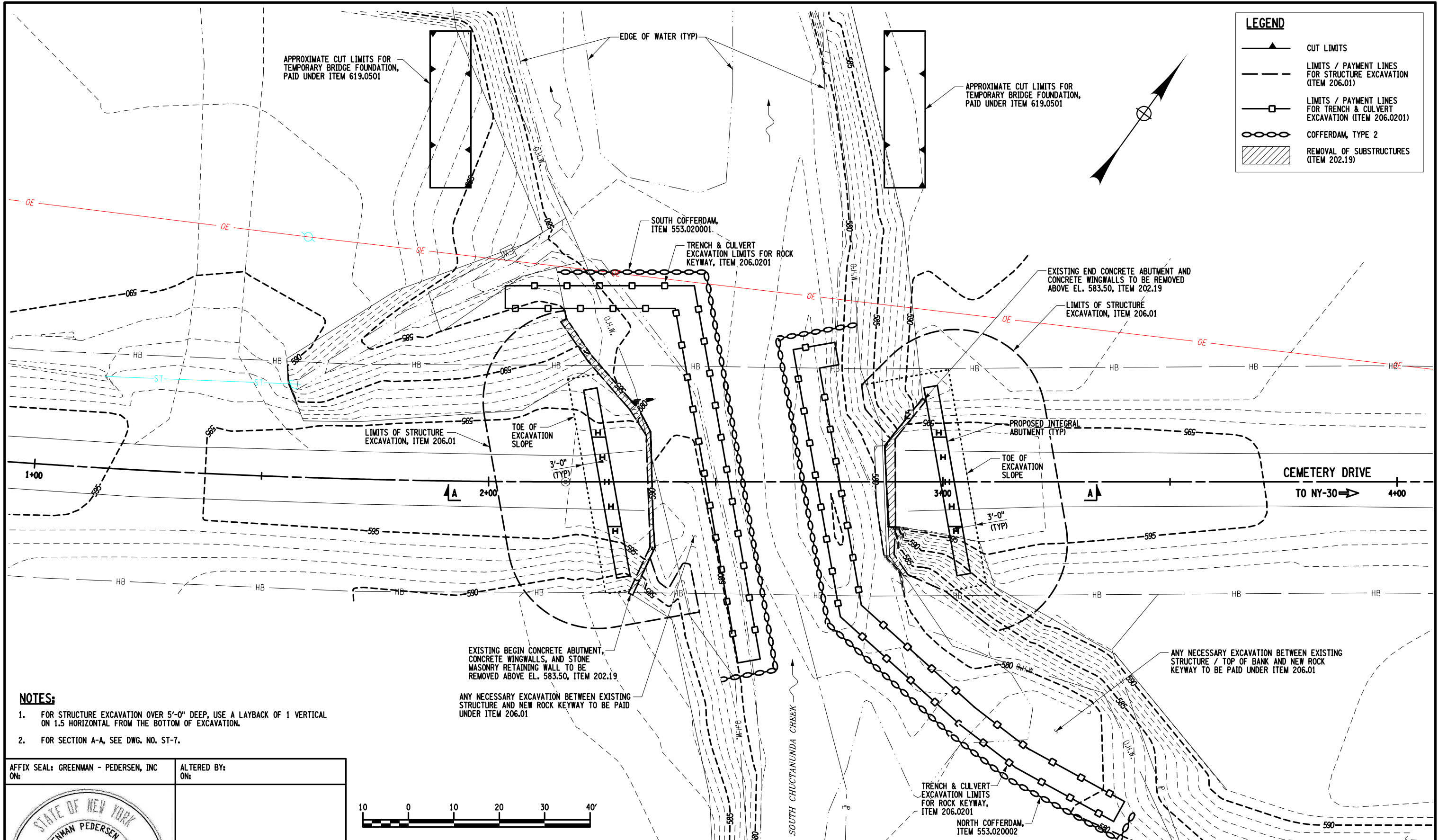
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| AS-BUILT REVISIONS<br>DESCRIPTION OF ALTERATIONS:  | CEMETERY DRIVE OVER THE                    | PIN 2754.54 | BRIDGES<br>3310200 | CULVERTS | ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED | CONTRACT NUMBER<br>D036212                                 |   |
|  | SOUTH CHUCTANUNDA CREEK BRIDGE REPLACEMENT |             |                    |          |   | EXISTING ABUTMENT ELEVATIONS                               | DRAWING NO. ST-4<br>SHEET NO. 21                |
|  | TOWN OF FLORIDA                            |             |                    |          |   |  |   |
|  | COUNTY: MONTGOMERY                         | REGION: 2   |                    |          |   |  |   |
| IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION. |  |             |                    |          |   | <b>GPI</b> GREENMAN-PEDERSEN, INC.<br>CONSULTING ENGINEERS | MONTGOMERY COUNTY<br>DEPARTMENT OF PUBLIC WORKS |

FILE NAME = \\CADD\275454\_CPB\_EXC\_PLN.dgn  
 DATE/TIME = 4/13/2022  
 USER = skern

DESIGN SUPERVISOR J. SIMKULET  
 JOB MANAGER D. THOMAS  
 DESIGN S. KERN  
 CHECK B. WEAVER  
 DRAFTING F. CUOCCIO  
 CHECK S. KERN  
 PROJECT MANAGER S. KERN

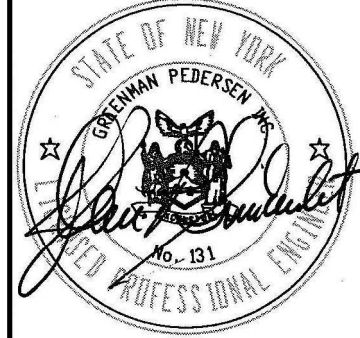


**LEGEND**

|  |  |
|--|--|
|  | CUT LIMITS   |
|  | LIMITS / PAYMENT LINES FOR STRUCTURE EXCAVATION (ITEM 206.01)          |
|  | LIMITS / PAYMENT LINES FOR TRENCH & CULVERT EXCAVATION (ITEM 206.0201) |
|  | COFFERDAM, TYPE 2  |
|  | REMOVAL OF SUBSTRUCTURES (ITEM 202.19)                                 |

- NOTES:**
- FOR STRUCTURE EXCAVATION OVER 5'-0" DEEP, USE A LAYBACK OF 1 VERTICAL ON 1.5 HORIZONTAL FROM THE BOTTOM OF EXCAVATION.
  - FOR SECTION A-A, SEE DWG. NO. ST-7.

AFFIX SEAL: GREENMAN - PEDERSEN, INC  
 ON: \_\_\_\_\_  
 ALTERED BY:  
 ON: \_\_\_\_\_

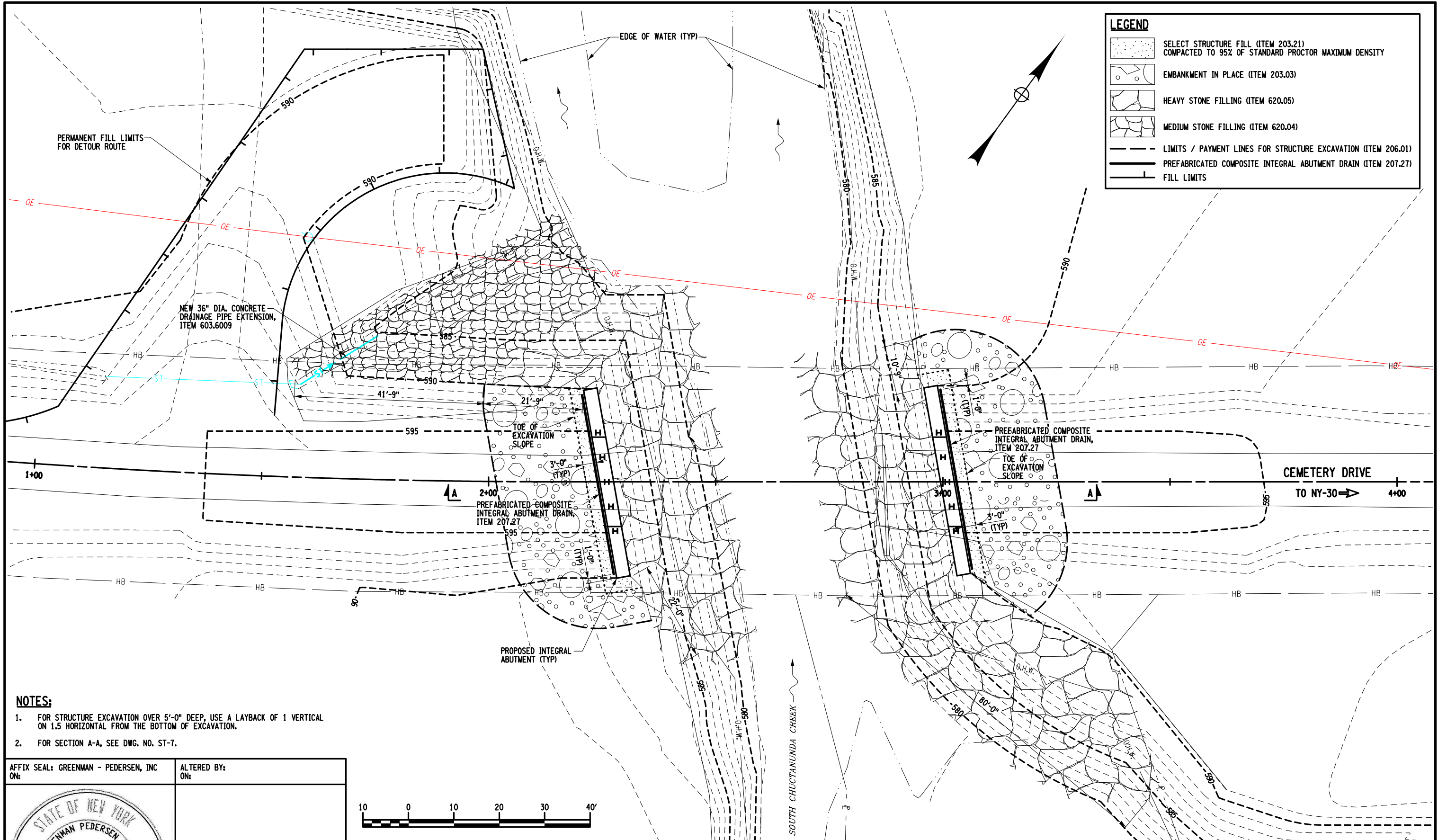


|  |  |             |                 |          |   |   |   |
|--|--|-------------|-----------------|----------|---|---|---|
| AS-BUILT REVISIONS<br>DESCRIPTION OF ALTERATIONS:  | CEMETERY DRIVE OVER THE                    | PIN 2754.54 | BRIDGES 3310200 | CULVERTS | ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED | CONTRACT NUMBER<br>D036212                      |   |
|  | SOUTH CHUCTANUNDA CREEK BRIDGE REPLACEMENT |             |                 |          |   | DRAWING NO. ST-5                                |   |
|  | TOWN OF FLORIDA                            | REGION: 2   |                 |          |   | SHEET NO. 22                                    |   |
| IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION. |  |             |                 |          |   | GREENMAN-PEDERSEN, INC.<br>CONSULTING ENGINEERS | MONTGOMERY COUNTY<br>DEPARTMENT OF PUBLIC WORKS |



FILE NAME = \\CADD\275454\_CPB\_ERW\_PLN.dgn  
 DATE/TIME = 4/13/2022  
 USER = skern

DESIGN SUPERVISOR J. SIMKULET  
 JOB MANAGER D. THOMAS  
 DESIGN S. KERN  
 CHECK B. WEAVER  
 DRAFTING F. CIUCCIO  
 CHECK S. KERN  
 PROJECT MANAGER S. KERN

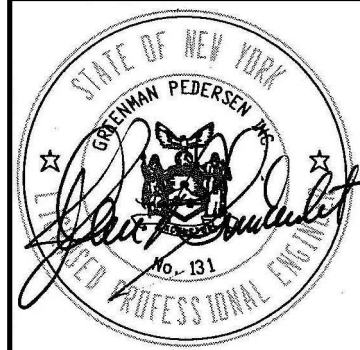


**LEGEND**

- SELECT STRUCTURE FILL (ITEM 203.21) COMPACTED TO 95% OF STANDARD PROCTOR MAXIMUM DENSITY
- EMBANKMENT IN PLACE (ITEM 203.03)
- HEAVY STONE FILLING (ITEM 620.05)
- MEDIUM STONE FILLING (ITEM 620.04)
- LIMITS / PAYMENT LINES FOR STRUCTURE EXCAVATION (ITEM 206.01)
- PREFABRICATED COMPOSITE INTEGRAL ABUTMENT DRAIN (ITEM 207.27)
- FILL LIMITS

- NOTES:**
- FOR STRUCTURE EXCAVATION OVER 5'-0" DEEP, USE A LAYBACK OF 1 VERTICAL ON 1.5 HORIZONTAL FROM THE BOTTOM OF EXCAVATION.
  - FOR SECTION A-A, SEE DWG. NO. ST-7.

AFFIX SEAL: GREENMAN - PEDERSEN, INC  
 ON: \_\_\_\_\_  
 ALTERED BY:  
 ON: \_\_\_\_\_



|   |  |             |                 |          |   |  |
|---|--|-------------|-----------------|----------|---|--|
| AS-BUILT REVISIONS<br>DESCRIPTION OF ALTERATIONS: | CEMETERY DRIVE OVER THE                    | PIN 2754.54 | BRIDGES 3310200 | CULVERTS | ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED | CONTRACT NUMBER<br>D036212   |
|   | SOUTH CHUCTANUNDA CREEK BRIDGE REPLACEMENT |             |                 |          |   | BACKFILL PLAN  |
| TOWN OF FLORIDA                                   |  | REGION: 2   |                 |          |   | GREENMAN-PEDERSEN, INC.<br>CONSULTING ENGINEERS          MONTGOMERY COUNTY<br>DEPARTMENT OF PUBLIC WORKS |
| COUNTY: MONTGOMERY                                |  |             |                 |          |   |  |

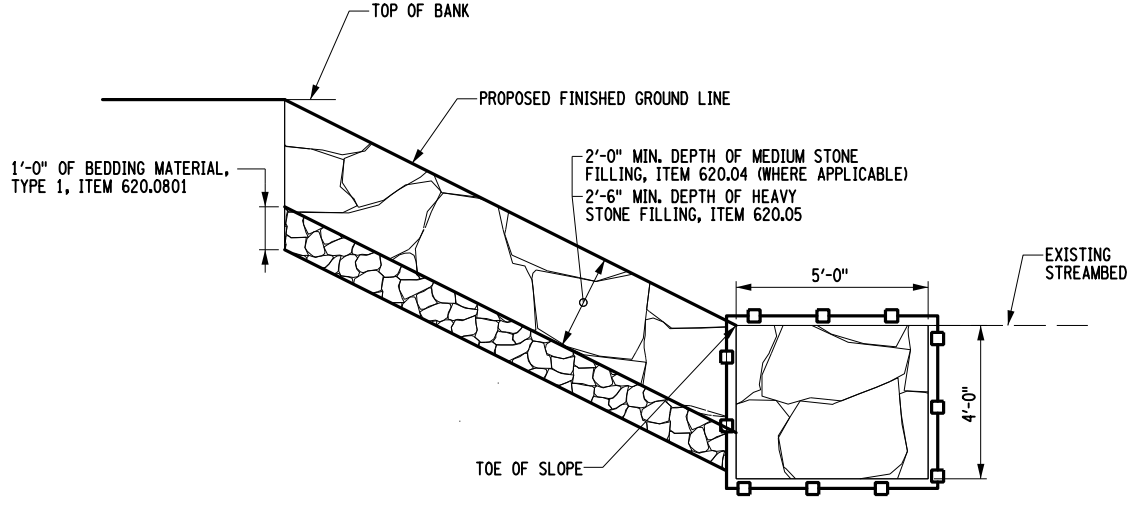
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.



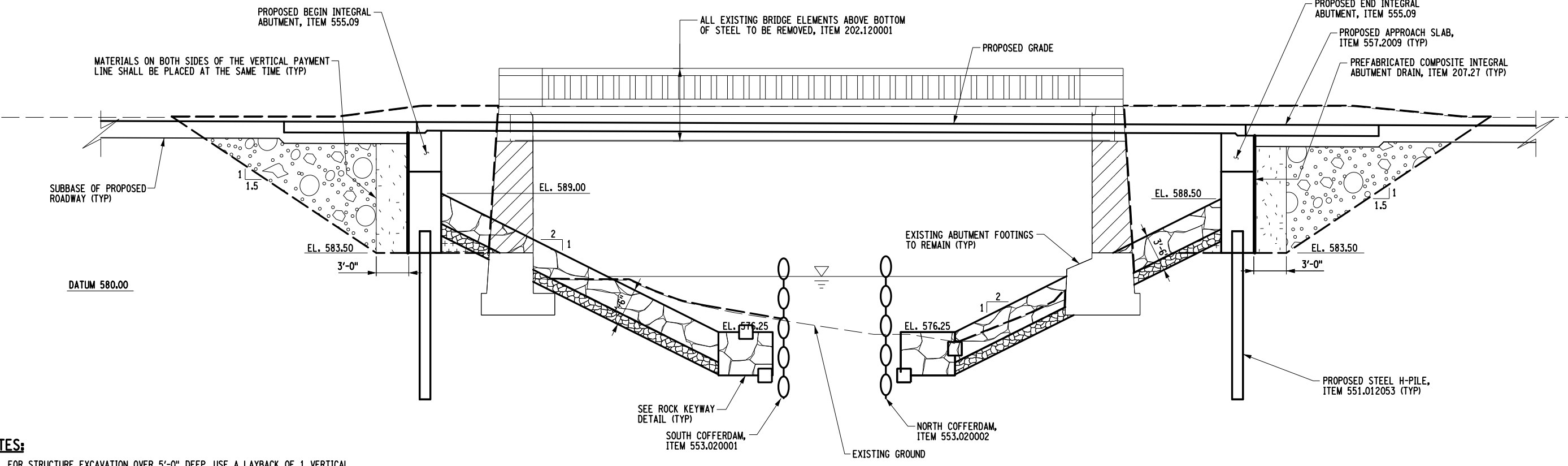
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 DATE/TIME = 4/13/2022  
 USER = skern

DESIGN SUPERVISOR J. SIMKULET  
 JOB MANAGER D. THOMAS  
 DESIGN S. KERN  
 CHECK B. WEAVER  
 DRAFTING F. CIUCCIO  
 CHECK S. KERN  
 PROJECT MANAGER S. KERN

| LEGEND |   |
|--------|---|
|        | REMOVAL OF SUBSTRUCTURES (ITEM 202.19)  |
|        | SELECT STRUCTURE FILL (ITEM 203.21)<br>COMPACTED TO 95% OF STANDARD PROCTOR MAXIMUM DENSITY       |
|        | EMBANKMENT IN PLACE (ITEM 203.03)   |
|        | HEAVY STONE FILLING (ITEM 620.05)   |
|        | BEDDING MATERIAL, TYPE 1 (ITEM 620.0801)  |
|        | BACKFILL WITH SUITABLE EXCAVATED MATERIAL AS PROVIDED UNDER<br>STRUCTURE EXCAVATION (ITEM 206.01) |
|        | LIMITS / PAYMENT LINES FOR STRUCTURE EXCAVATION (ITEM 206.01)                                     |
|        | PREFABRICATED COMPOSITE INTEGRAL ABUTMENT DRAIN (ITEM 207.27)                                     |
|        | LIMITS / PAYMENT LINES FOR TRENCH & CULVERT EXCAVATION (ITEM 206.0201)                            |
|        | COFFERDAM, TYPE 2   |



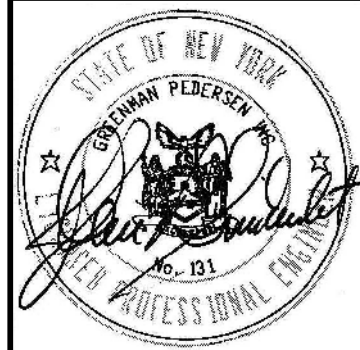
**ROCK KEYWAY DETAIL**  
 SCALE: 1"=5'-0"



**SECTION A-A**  
 SCALE: 1"=10'-0"

- NOTES:**
- FOR STRUCTURE EXCAVATION OVER 5'-0" DEEP, USE A LAYBACK OF 1 VERTICAL ON 1.5 HORIZONTAL FROM THE BOTTOM OF EXCAVATION.
  - FOR SECTION A-A, SEE DWG. NO. ST-5 AND ST-6.

AFFIX SEAL: GREENMAN - PEDERSEN, INC  
 ON: \_\_\_\_\_  
 ALTERED BY:  
 ON: \_\_\_\_\_



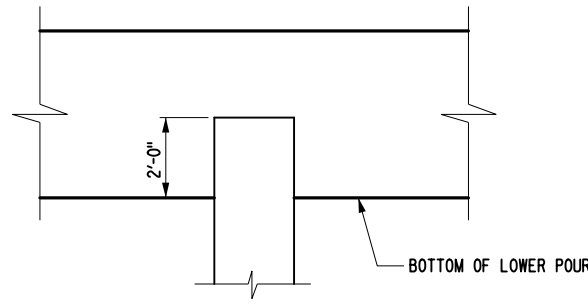
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| AS-BUILT REVISIONS<br>DESCRIPTION OF ALTERATIONS: | CEMETERY DRIVE OVER THE                    | PIN 2754.54 | BRIDGES 3310200 | CULVERTS | ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED | CONTRACT NUMBER<br>D036212 |                  |
|   | SOUTH CHUCTANUNDA CREEK BRIDGE REPLACEMENT |             |                 |          |   | EARTHWORK SECTIONS         | DRAWING NO. ST-7 |
|   | TOWN OF FLORIDA                            |             |                 |          |   |                            | SHEET NO. 24     |
|   | COUNTY: MONTGOMERY                         | REGION: 2   |                 |          |   |                            |                  |

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

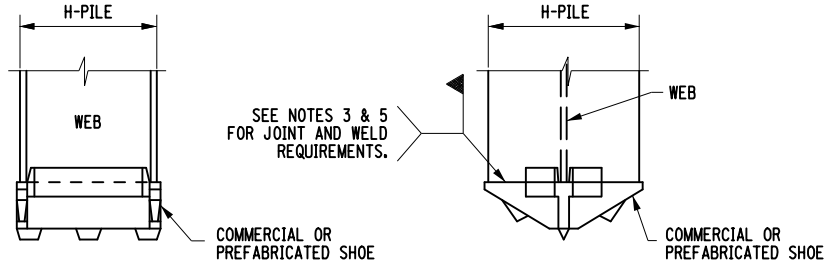
**GPI** GREENMAN-PEDERSEN, INC. CONSULTING ENGINEERS  
 MONTGOMERY COUNTY DEPARTMENT OF PUBLIC WORKS

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 USER = skern

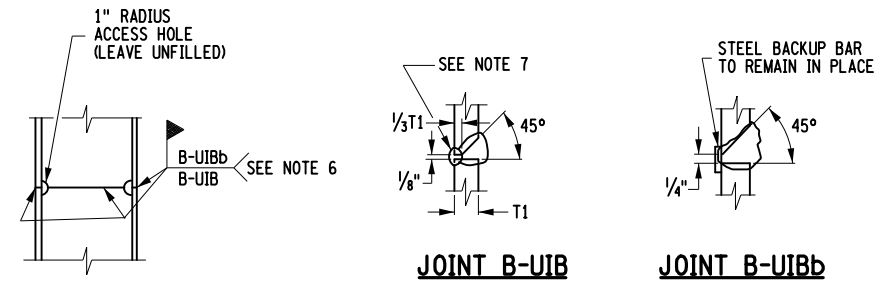
PROJECT MANAGER S. KERN  
 CHECK S. KERN  
 DRAFTING F. CUOCCIO  
 CHECK B. WEAVER  
 DESIGN S. KERN  
 JOB MANAGER D. THOMAS  
 DESIGN SUPERVISOR J. SIMKULET



**BEARING PILE EMBEDMENT**  
 NOT TO SCALE



**STEEL BEARING PILE SHOES**  
 NOT TO SCALE



**SPLICE FOR STEEL BEARING PILE**  
 NOT TO SCALE

| BEGIN ABUTMENT PILE TABLE |                        |
|---------------------------|------------------------|
| PILE NO.                  | INSTALLED LENGTH (FT)* |
| B1                        |                        |
| B2                        |                        |
| B3                        |                        |
| B4                        |                        |
| B5                        |                        |

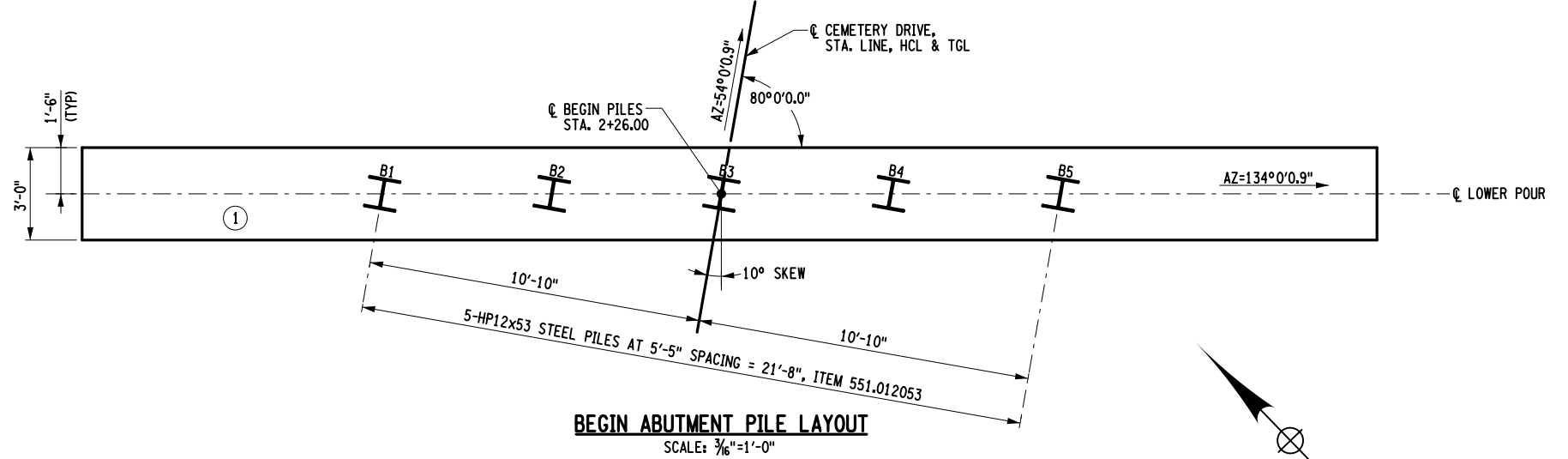
| END ABUTMENT PILE TABLE |                        |
|-------------------------|------------------------|
| PILE NO.                | INSTALLED LENGTH (FT)* |
| E1                      |                        |
| E2                      |                        |
| E3                      |                        |
| E4                      |                        |
| E5                      |                        |

\* LENGTH BELOW CUT-OFF ELEVATION OF 585.50. ESTIMATED LENGTH IS 24'-0".

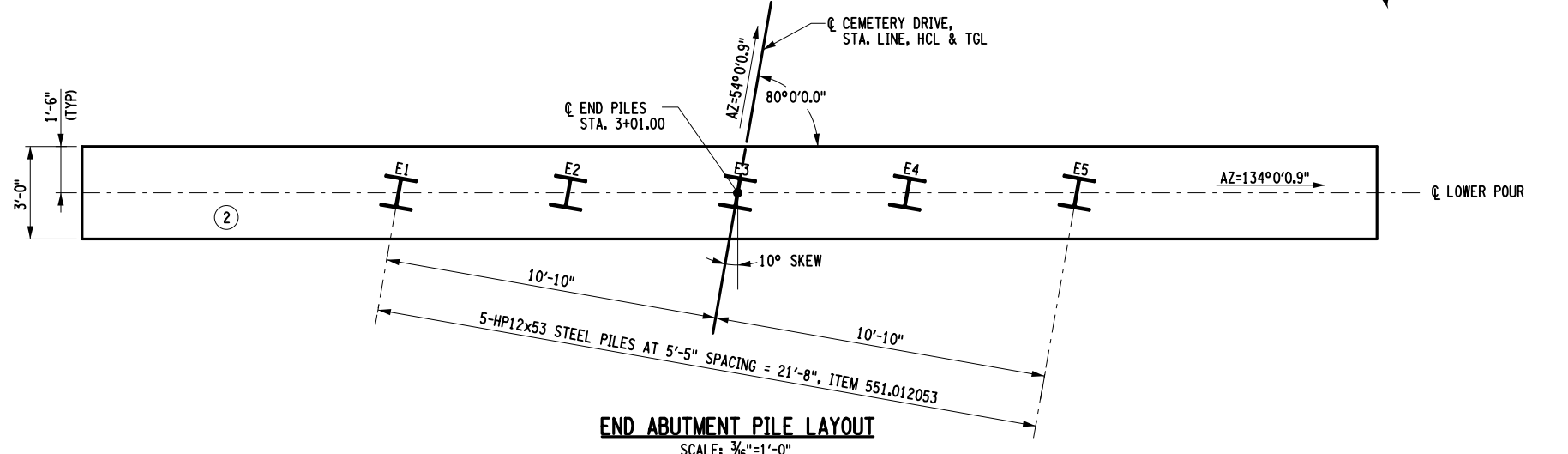
\* LENGTH BELOW CUT-OFF ELEVATION OF 585.50. ESTIMATED LENGTH IS 50'-0".

**NOTES:**

1. COMMERCIAL OR PREFABRICATED SHOES ARE SUBJECT TO THE APPROVAL OF THE RESIDENT ENGINEER.
2. A "WELDING PROCEDURE SPECIFICATION" (WPS) APPROVED BY THE RESIDENT ENGINEER IS REQUIRED.
3. THE SHOE WELD JOINT DESIGN SHALL BE DONE IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATION, AND AS SHOWN ON THE APPROVED WPS.
4. IF SHOES ARE WELDED AT A LOCATION OTHER THAN THE PROJECT SITE, ALL OF THE ABOVE PROVISIONS SHALL APPLY TO THE OFF-SITE FABRICATOR. THE RESIDENT ENGINEER SHALL BE NOTIFIED BY THE CONTRACTOR OF THE ACTUAL LOCATION WHERE THE WELDING WILL BE PERFORMED A MINIMUM OF 5 WORKING DAYS BEFORE WORK COMMENCES.
5. ALL WELDING SHALL BE PERFORMED BY A CERTIFIED WELDER IN CONFORMANCE WITH REQUIREMENTS FOR WELDING SPECIFIED IN THE N.Y.S. STEEL CONSTRUCTION MANUAL.
6. JOINT B: EITHER JOINT MAY BE USED AT CONTRACTOR'S OPTION.
7. B-UIB: AIR CARBON ARC GOUGE TO SOUND WELD METAL PRIOR TO WELDING THE SECOND SIDE. THE GOUGE SHALL HAVE A 1/4" MINIMUM RADIUS AT THE ROOT WITH THE TOP SLOPED BACK AT 45° MINIMUM.
8. THE BEGIN ABUTMENT PILES ARE DESIGNED TO SUPPORT A MAXIMUM STRENGTH LIMIT STATE AXIAL LOAD OF 160 KIPS PER PILE. DRIVE THESE PILES TO ACHIEVE A NOMINAL RESISTANCE OF 328 KIPS PER PILE.
9. THE MAXIMUM SERVICE LIMIT STATE AXIAL LOAD APPLIED TO THE PILES AT THE BEGIN ABUTMENT IS 191 KIPS PER PILE.
10. THE END ABUTMENT PILES ARE DESIGNED TO SUPPORT A MAXIMUM STRENGTH LIMIT STATE AXIAL LOAD OF 165 KIPS PER PILE. DRIVE THESE PILES TO ACHIEVE A NOMINAL RESISTANCE OF 384 KIPS PER PILE.
11. THE MAXIMUM SERVICE LIMIT STATE AXIAL LOAD APPLIED TO THE PILES AT THE END ABUTMENT IS 255 KIPS PER PILE.
12. DRIVE THE PILES TO A MINIMUM LENGTH OF 10 FEET. THE RESIDENT ENGINEER WILL IMMEDIATELY CONTACT THE ENGINEER IF THE MINIMUM LENGTH IS NOT ACHIEVED.
13. DIFFICULT DRIVING OF PILES MAY BE ENCOUNTERED AND IT MAY BE NECESSARY TO USE MECHANICAL EQUIPMENT TO REMOVE VERY COMPACT MATERIAL OR BouldERS FROM THE LOCATION OF THE PILES. WHEN REQUIRED, SPUD OR EXCAVATE HOLES PRIOR TO DRIVING IN ACCORDANCE WITH SECTION 551. TO BE INCLUDED IN THE PRICE BID FOR DRIVING PILES, ITEM 551.13.
14. EQUIP ALL STEEL H-PILES WITH REINFORCED SHOES TO BE PAID UNDER ITEM 551.012053.
15. ALL STEEL H-PILES ARE TO BE PAID UNDER ITEM 551.012053. ALL COST ASSOCIATED WITH DRIVING PILES ARE TO BE PAID UNDER ITEM 551.13.



**BEGIN ABUTMENT PILE LAYOUT**  
 SCALE: 3/16"=1'-0"



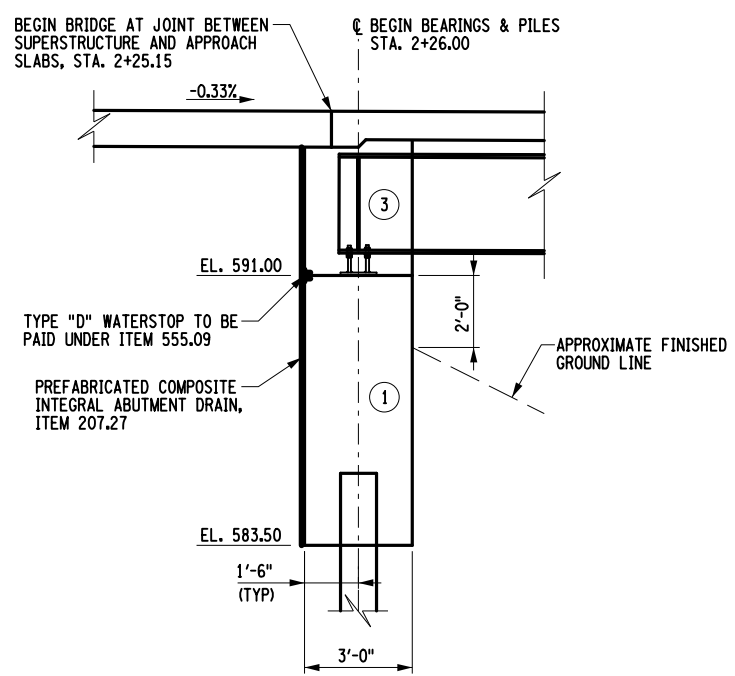
**END ABUTMENT PILE LAYOUT**  
 SCALE: 3/16"=1'-0"

AFFIX SEAL: GREENMAN - PEDERSEN, INC  
 ON:

ALTERED BY:  
 ON:

|  |  |             |                 |          |   |  |
|--|--|-------------|-----------------|----------|---|--|
| AS-BUILT REVISIONS<br>DESCRIPTION OF ALTERATIONS:  | CEMETERY DRIVE OVER THE                    | PIN 2754.54 | BRIDGES 3310200 | CULVERTS | ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED | CONTRACT NUMBER D036212                      |
|  | SOUTH CHUCTANUNDA CREEK BRIDGE REPLACEMENT |             |                 |          |   | DRAWING NO. ST-8                             |
|  | TOWN OF FLORIDA                            | REGION: 2   |                 |          | PILE LAYOUT                                 | SHEET NO. 25                                 |
| IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION. |  |             |                 |          |   | MONTGOMERY COUNTY DEPARTMENT OF PUBLIC WORKS |

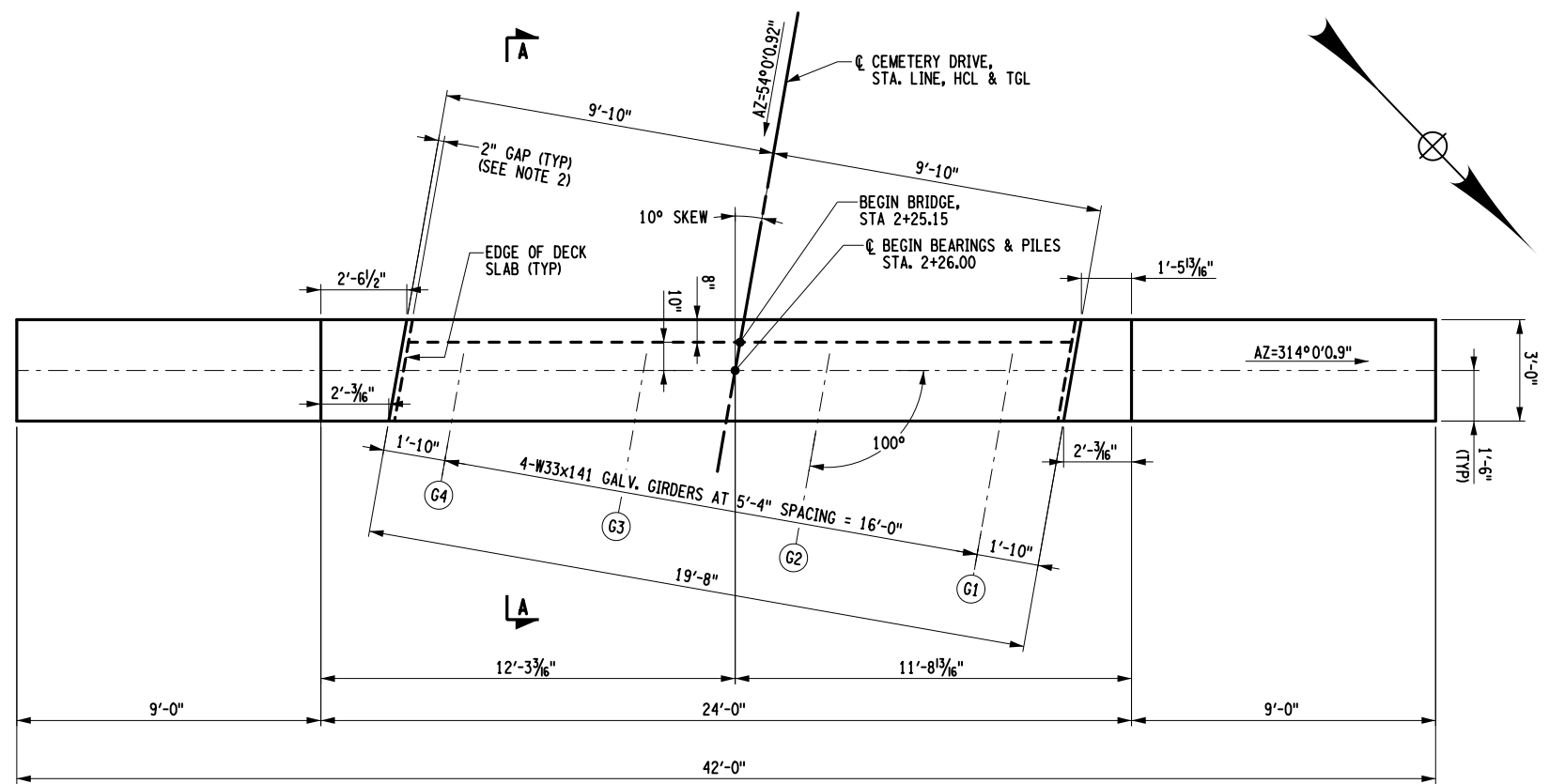
PROJECT MANAGER S. KERN  
 CHECK S. KERN  
 DRAFTING F. CIUCCIO  
 CHECK B. WEAVER  
 DESIGN S. KERN  
 JOB MANAGER D. THOMAS  
 DESIGN SUPERVISOR J. SIMKULET



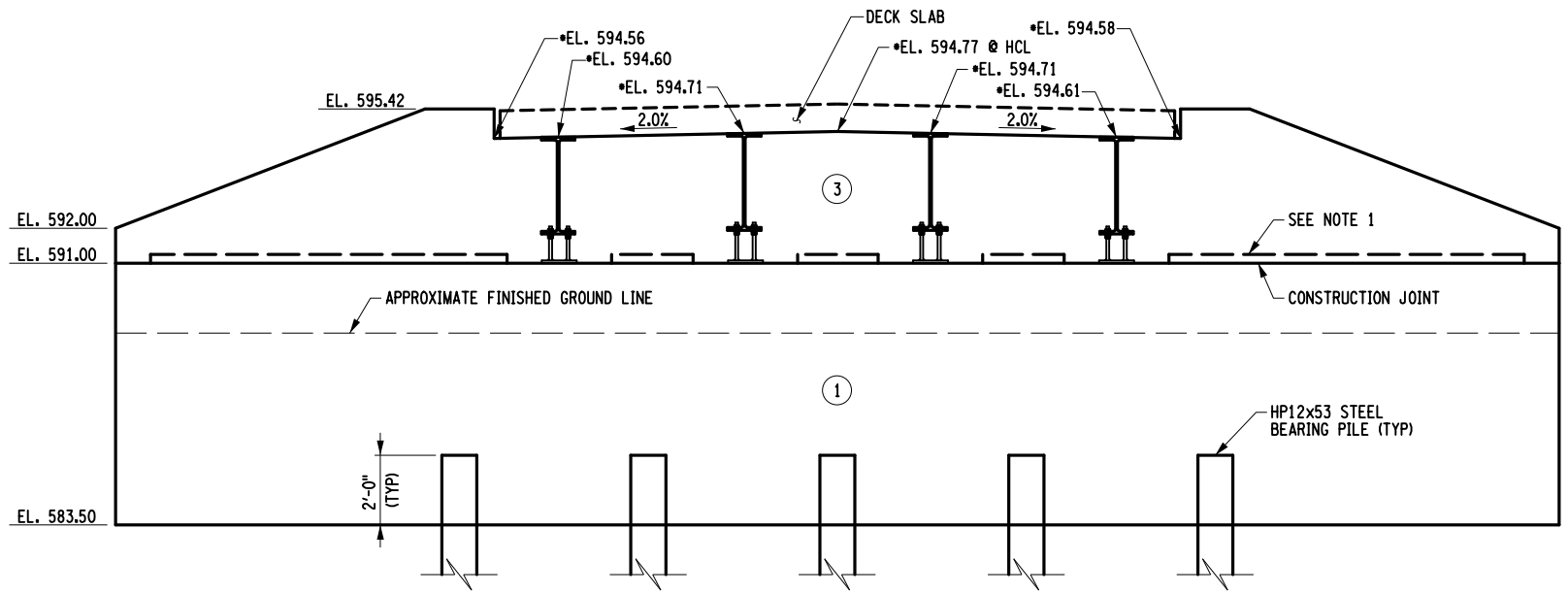
**SECTION A-A**  
SCALE: 3/16"=1'-0"

| CONCRETE TABLE |          |          |
|----------------|----------|----------|
| PLACEMENT      | QUANTITY | ITEM NO. |
| 1              | 35.0 CY  | 555.09   |
| 3              | 15.3 CY  | 555.09   |

- NOTES:**
- ALL HORIZONTAL KEYWAYS TO STOP 1'-0" FROM SUPPORT PLATE AND FACE OF CONCRETE POUR.
  - THE MINIMUM GAP BETWEEN THE DECK SLAB AND THE WINGWALL SHALL BE 2" AND FILLED WITH AT LEAST TWO SHEETS OF PREMOULDED RESILIENT JOINT FILLER, MATERIAL SUBSECTION 705-07. PAYMENT TO BE INCLUDED IN THE UNIT PRICE BID FOR THE APPROACH SLAB, ITEM 557.2009.

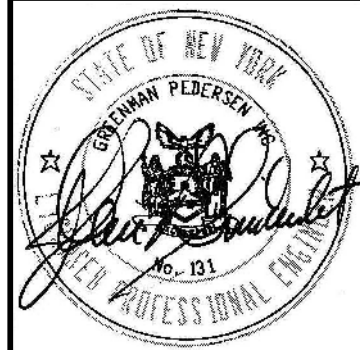


**BEGIN ABUTMENT PLAN**  
SCALE: 3/16"=1'-0"



**BEGIN ABUTMENT ELEVATION**  
SCALE: 3/16"=1'-0"

AFFIX SEAL: GREENMAN - PEDERSEN, INC  
ON: \_\_\_\_\_  
ALTERED BY: \_\_\_\_\_  
ON: \_\_\_\_\_

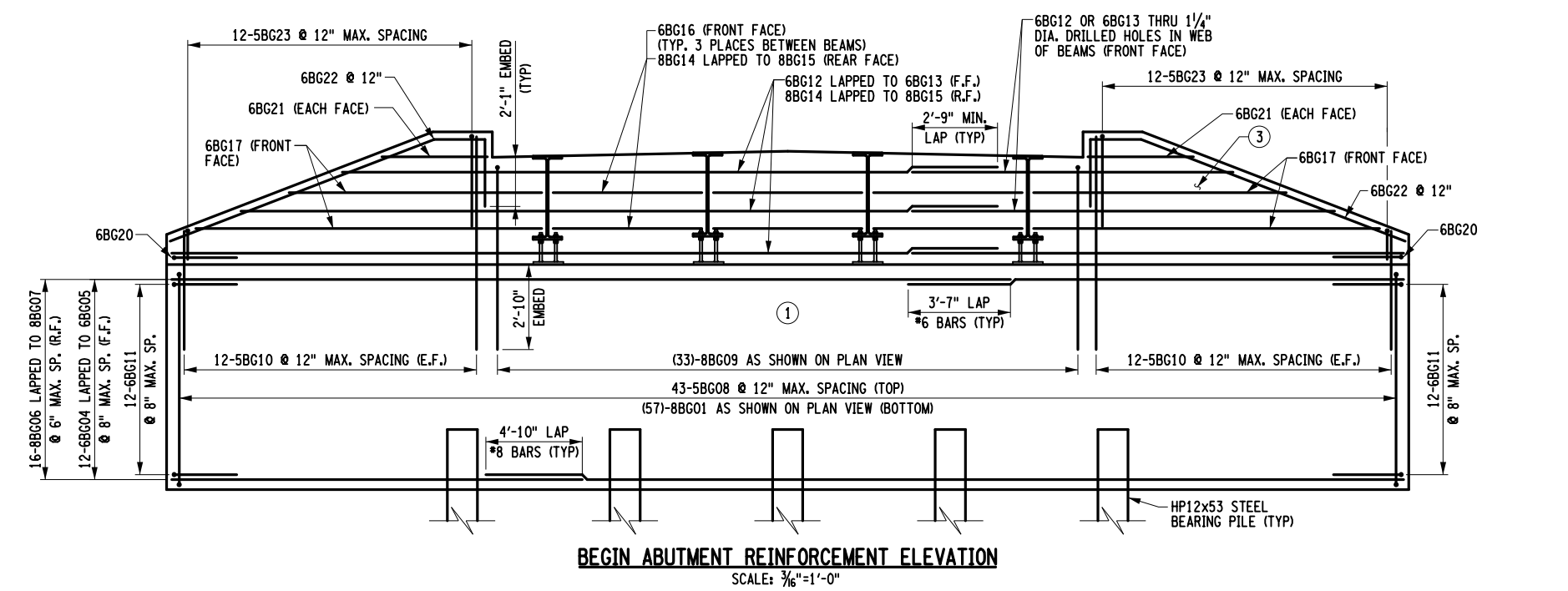
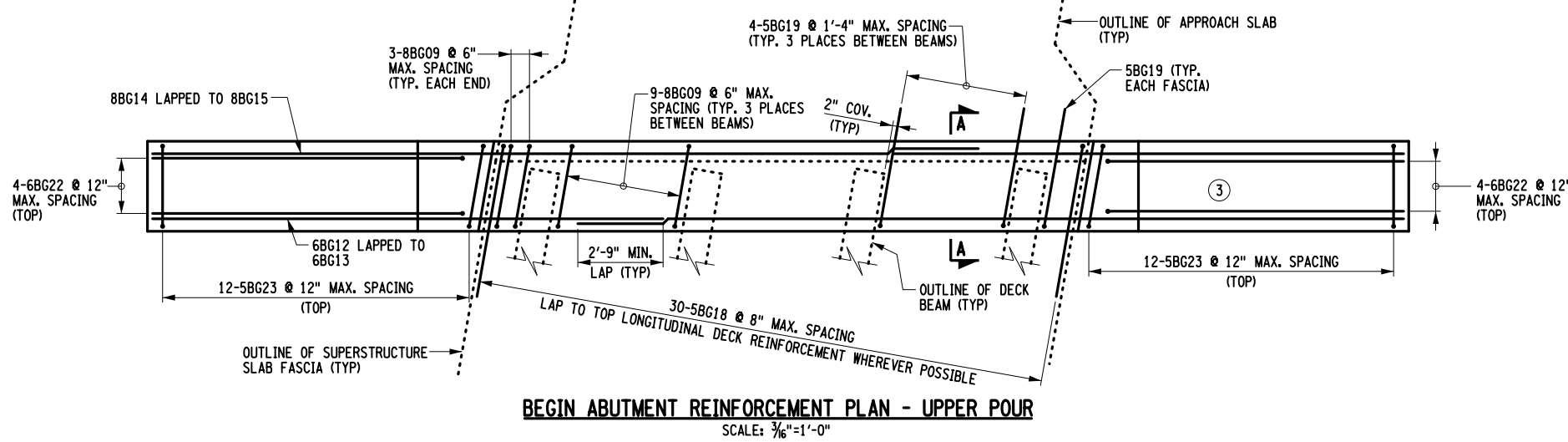
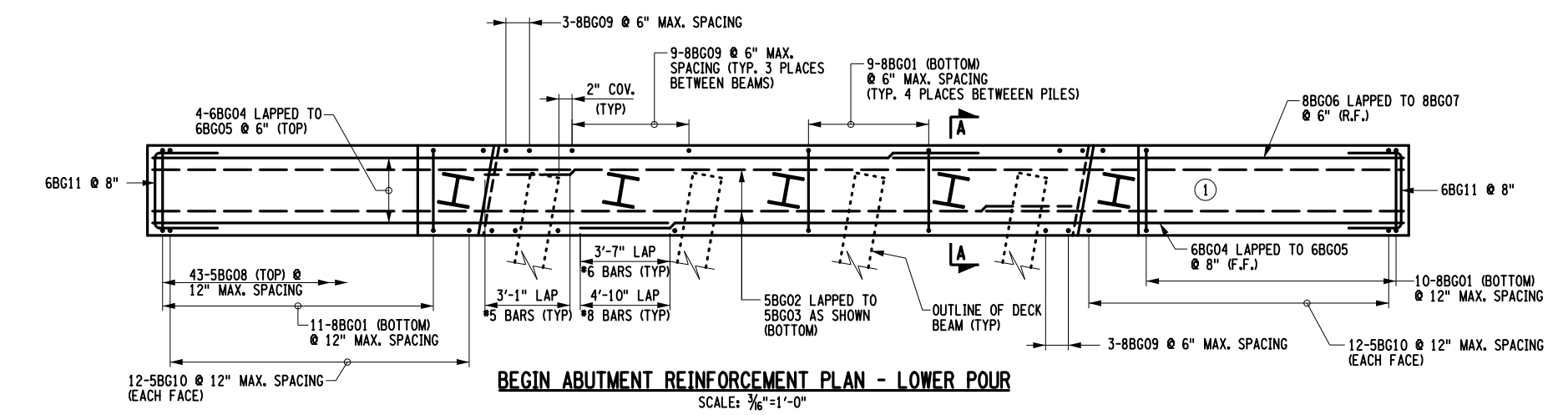
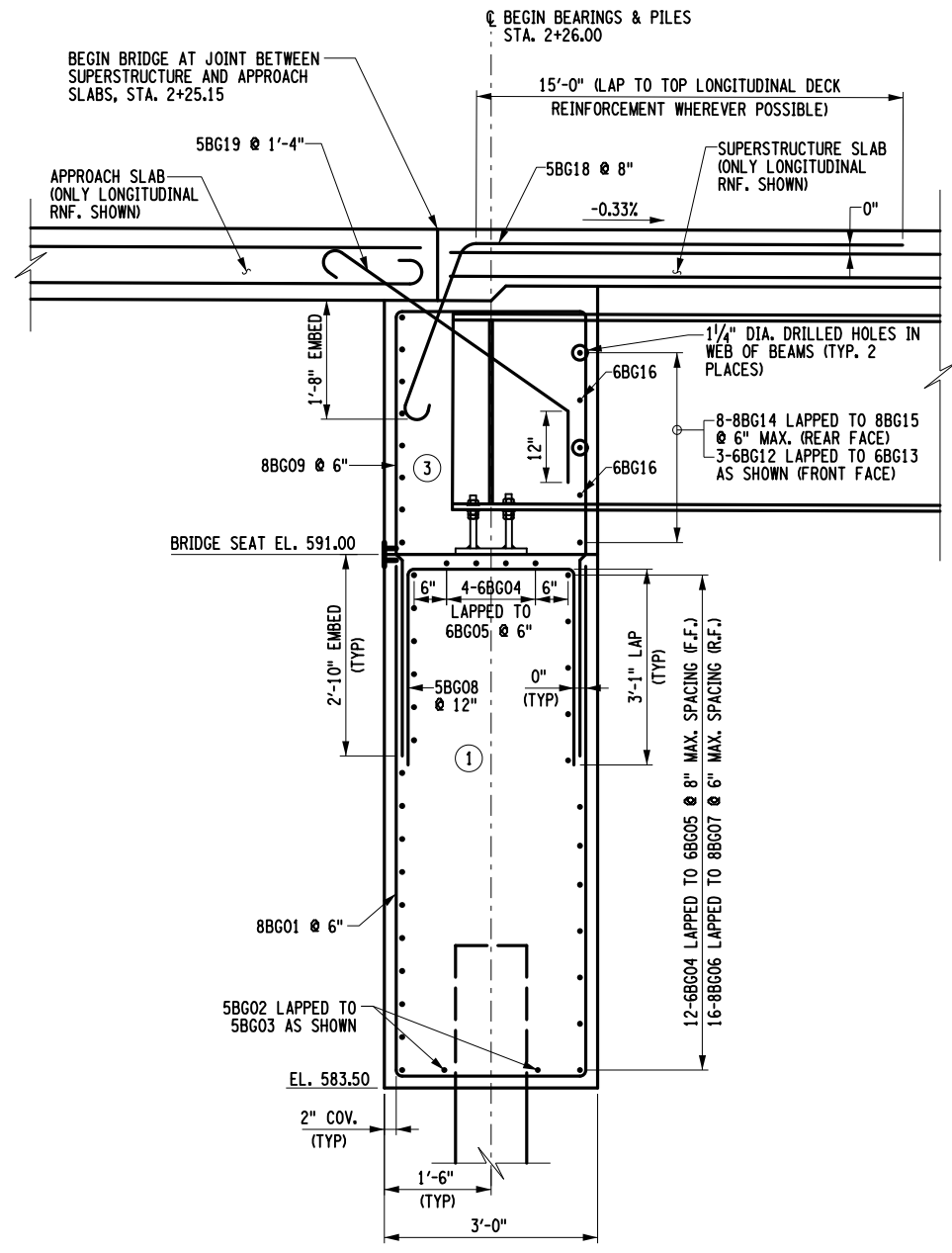


|  |  |                 |                 |          |   |                                    |
|--|--|-----------------|-----------------|----------|---|------------------------------------|
| AS-BUILT REVISIONS<br>DESCRIPTION OF ALTERATIONS:  | CEMETERY DRIVE OVER THE                    | PIN 2754.54     | BRIDGES 3310200 | CULVERTS | ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED | CONTRACT NUMBER<br>D036212         |
|  | SOUTH CHUCTANUNDA CREEK BRIDGE REPLACEMENT | TOWN OF FLORIDA | REGION: 2       |          |   | BEGIN ABUTMENT<br>PLAN & ELEVATION |
| IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION. |  |                 |                 |          |   |                                    |

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 USER = skern

FILE NAME = ...275454\_CPL\_ABT\_BGN.RNF.dgn  
 DATE/TIME = 4/13/2022  
 USER = skern

PROJECT MANAGER S. KERN  
 CHECK S. KERN  
 DRAFTING F. CUOCCIO  
 CHECK B. WEAVER  
 DESIGN S. KERN  
 JOB MANAGER D. THOMAS  
 DESIGN SUPERVISOR J. SIMKULET



**NOTES:**  
 1. REINFORCEMENT IS TO BE PAID UNDER ITEM 556.0203 - GALVANIZED BAR REINFORCEMENT.

AFFIX SEAL: GREENMAN - PEDERSEN, INC ON:  
 ALTERED BY: ON:



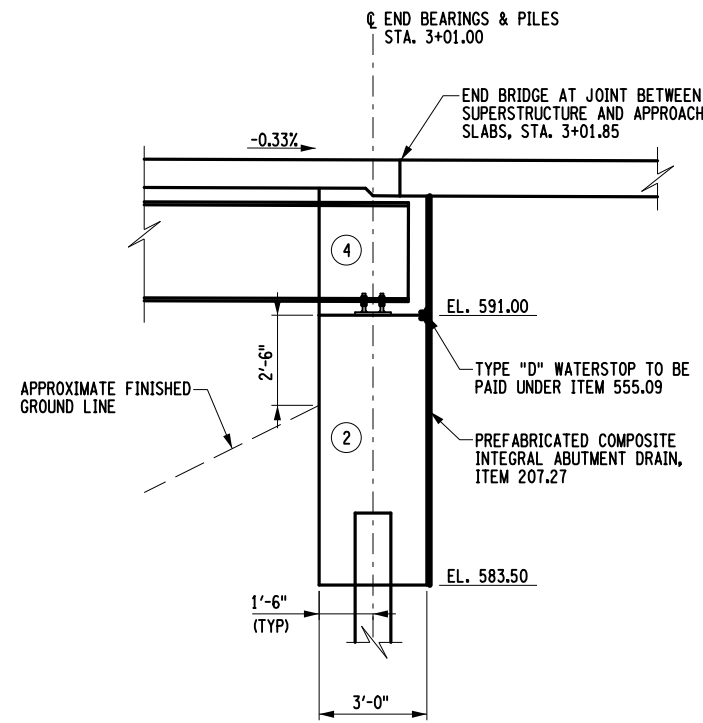
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| AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS: | CEMETERY DRIVE OVER THE SOUTH CHUCTANUNDA CREEK BRIDGE REPLACEMENT | PIN 2754.54 | BRIDGES 3310200 | CULVERTS | ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED | CONTRACT NUMBER D036212 |
|  | TOWN OF FLORIDA  |             |                 |          | BEGIN ABUTMENT REINFORCEMENT                | DRAWING NO. ST-10       |
|  | COUNTY: MONTGOMERY REGION: 2                                       |             |                 |          |   | SHEET NO. 27            |

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

**GPI** GREENMAN-PEDERSEN, INC. CONSULTING ENGINEERS  
 MONTGOMERY COUNTY DEPARTMENT OF PUBLIC WORKS

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 USER = skern

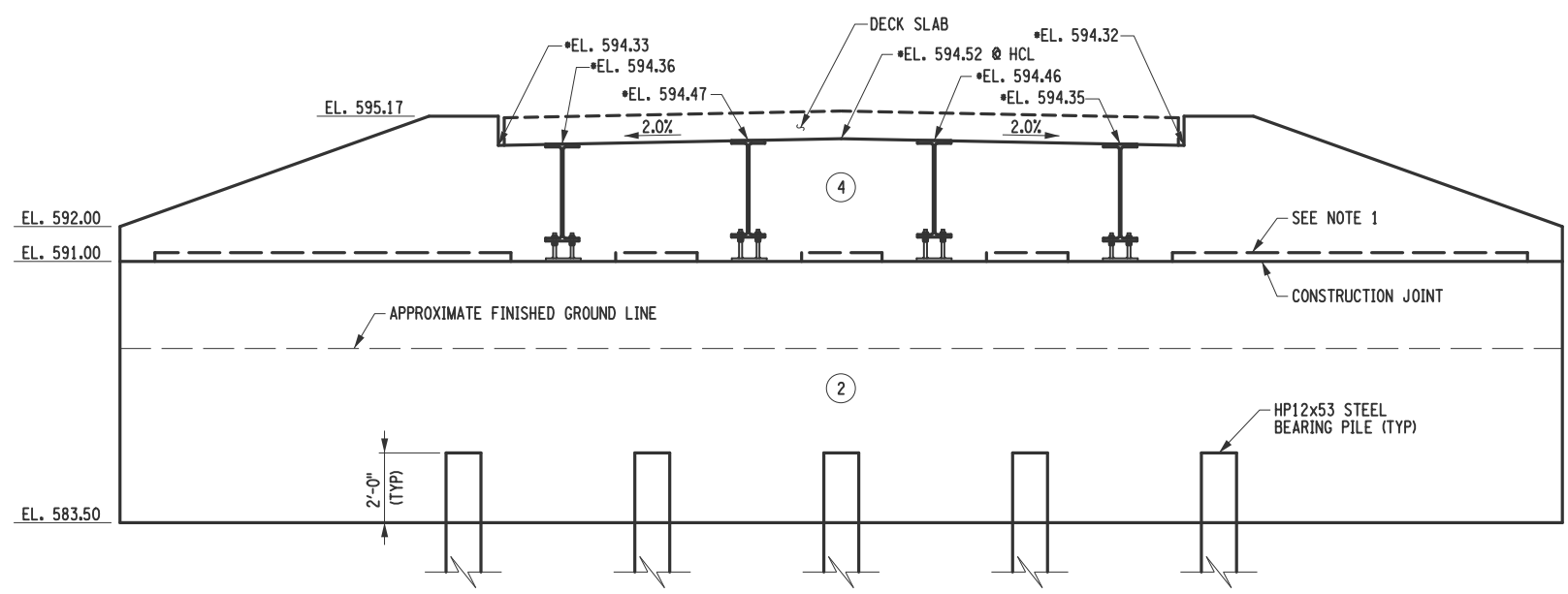
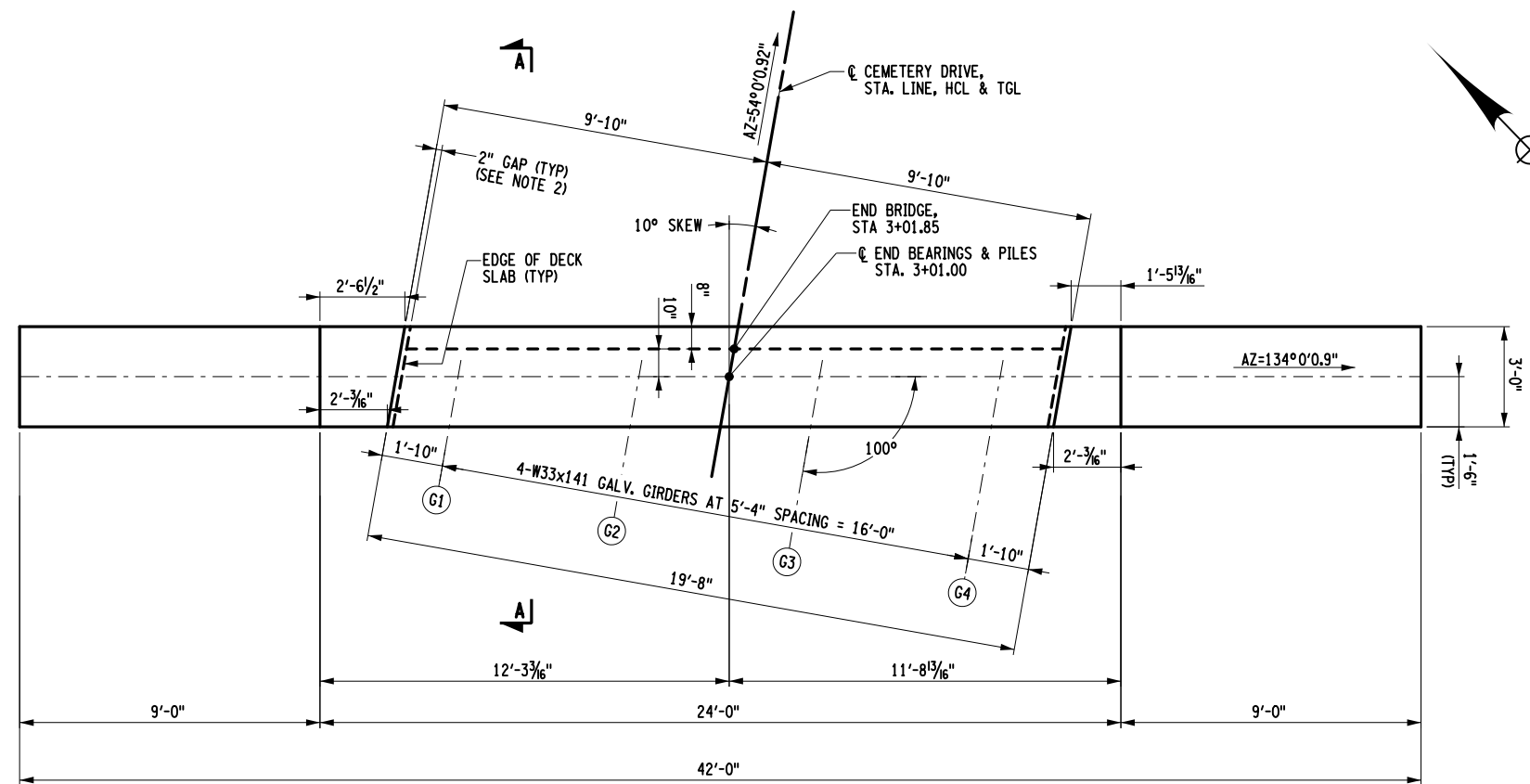
DESIGN SUPERVISOR J. SIMKULET  
 JOB MANAGER D. THOMAS  
 DESIGN S. KERN  
 DRAFTING F. CIUCCIO  
 CHECK B. WEAVER  
 CHECK S. KERN  
 PROJECT MANAGER S. KERN



| CONCRETE TABLE |          |          |
|----------------|----------|----------|
| PLACEMENT      | QUANTITY | ITEM NO. |
| 2              | 35.0 CY  | 555.09   |
| 4              | 15.3 CY  | 555.09   |

**NOTES:**

- ALL HORIZONTAL KEYWAYS TO STOP 1'-0" FROM SUPPORT PLATE AND FACE OF CONCRETE POUR.
- THE MINIMUM GAP BETWEEN THE DECK SLAB AND THE WINGWALL SHALL BE 2" AND FILLED WITH AT LEAST TWO SHEETS OF PREMOULDED RESILIENT JOINT FILLER, MATERIAL SUBSECTION 705-07, PAYMENT TO BE INCLUDED IN THE UNIT PRICE BID FOR THE APPROACH SLAB, ITEM 557.2009.



AFFIX SEAL: GREENMAN - PEDERSEN, INC ON: ALTERED BY: ON:

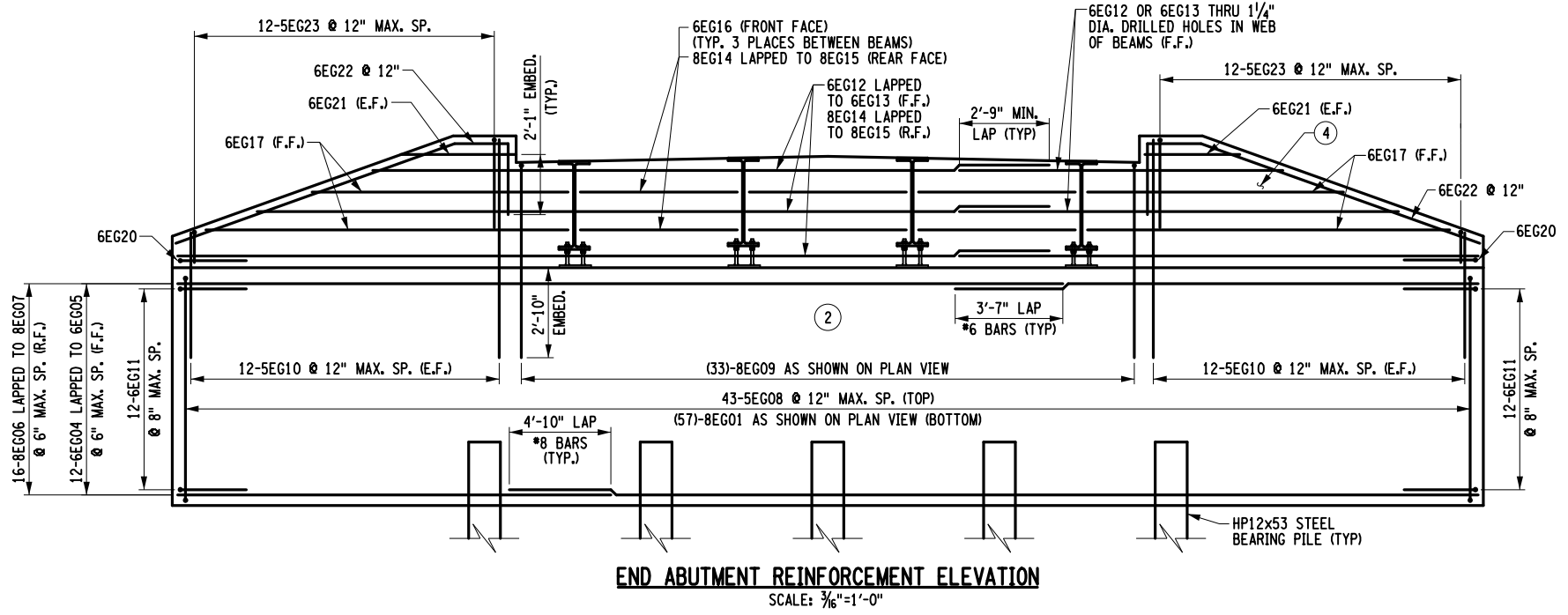
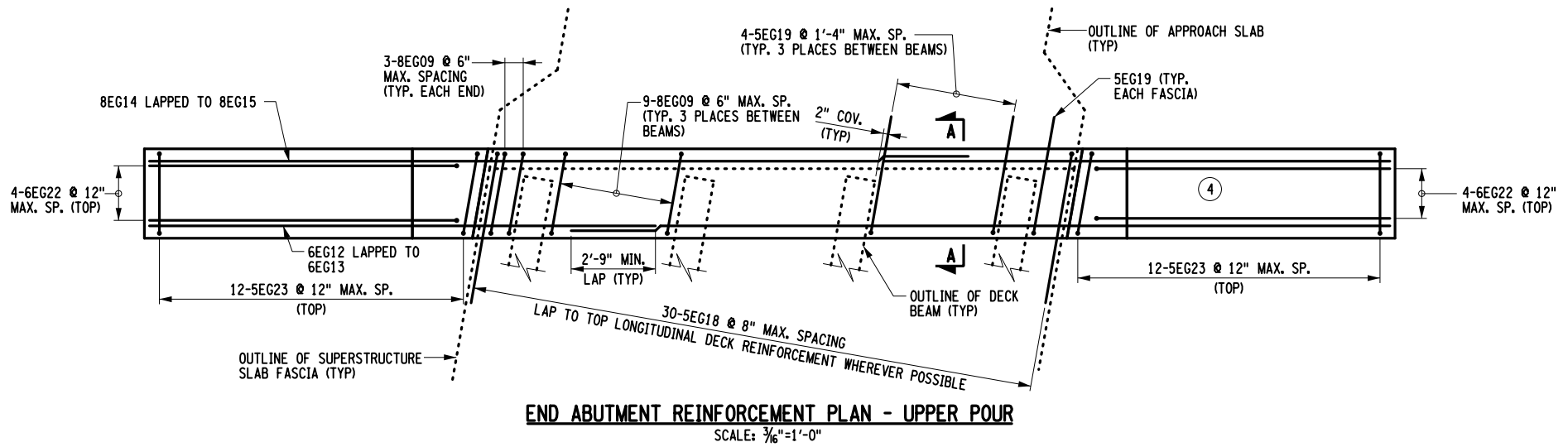
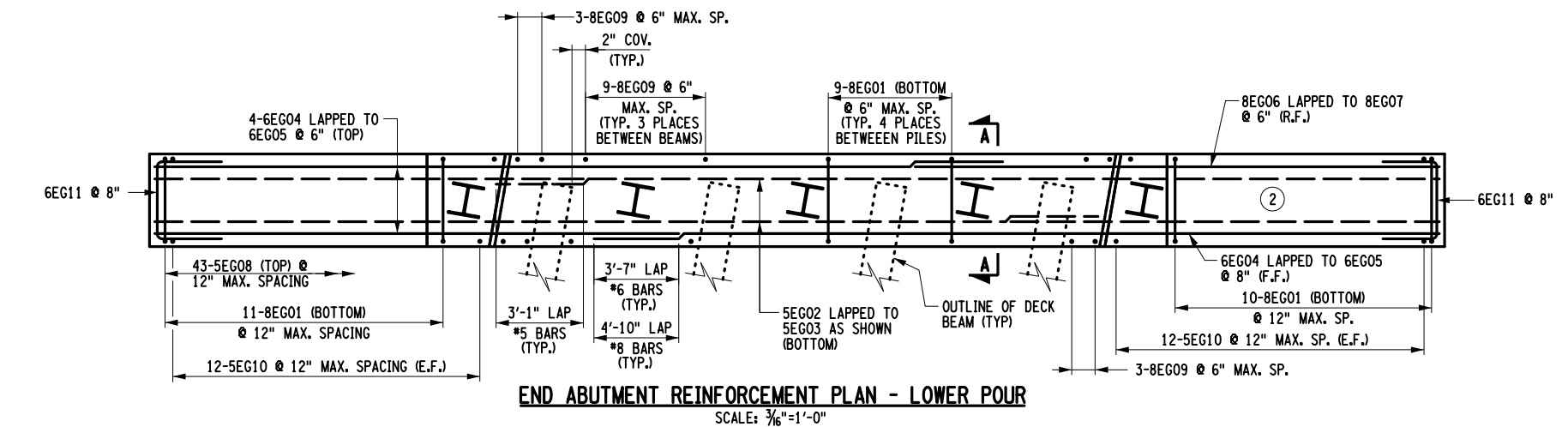
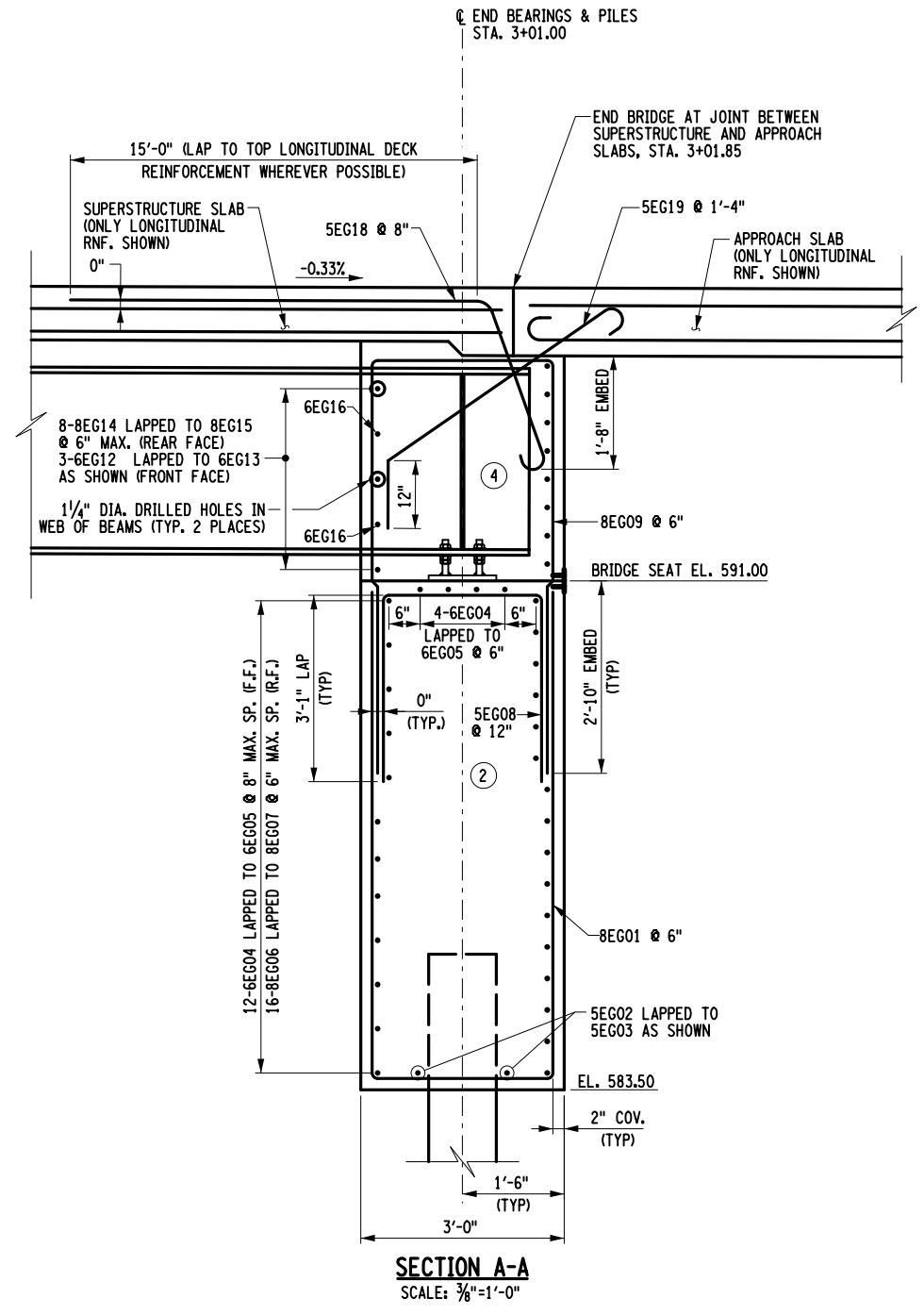


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|--|--|-----------------|-----------------|----------|---|---|
| AS-BUILT REVISIONS<br>DESCRIPTION OF ALTERATIONS:  | CEMETERY DRIVE OVER THE                    | PIN 2754.54     | BRIDGES 3310200 | CULVERTS | ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED         | CONTRACT NUMBER<br>D036212                      |
|  | SOUTH CHUCTANUNDA CREEK BRIDGE REPLACEMENT | TOWN OF FLORIDA | REGION: 2       |          |   | END ABUTMENT<br>PLAN & ELEVATION                |
| IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION. |  |                 |                 |          | GPI GREENMAN-PEDERSEN, INC.<br>CONSULTING ENGINEERS | MONTGOMERY COUNTY<br>DEPARTMENT OF PUBLIC WORKS |



FILE NAME = ...275454\_CPB\_ABT\_END\_RNF.dgn  
 DATE/TIME = 4/13/2022  
 USER = skern

DESIGN SUPERVISOR J. SIMKULET  
 JOB MANAGER D. THOMAS  
 DESIGN S. KERN  
 CHECK B. WEAVER  
 DRAFTING F. CUOCCIO  
 CHECK S. KERN  
 PROJECT MANAGER S. KERN



**NOTES:**  
 1. REINFORCEMENT IS TO BE PAID UNDER ITEM 556.0203 - GALVANIZED BAR REINFORCEMENT.

AFFIX SEAL: GREENMAN - PEDERSEN, INC ON: ALTERED BY: ON:



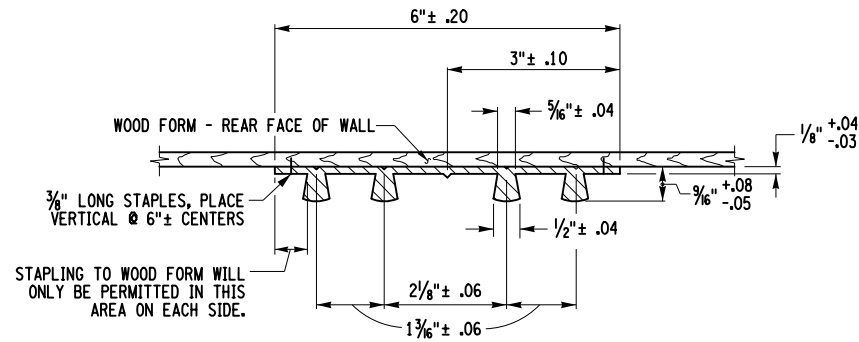
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| AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS: | CEMETERY DRIVE OVER THE SOUTH CHUCTANUNDA CREEK BRIDGE REPLACEMENT | PIN 2754.54 | BRIDGES 3310200 | CULVERTS | ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED | CONTRACT NUMBER D036212 |
|  | TOWN OF FLORIDA  |             |                 |          | END ABUTMENT REINFORCEMENT                  | DRAWING NO. ST-12       |
|  | COUNTY: MONTGOMERY REGION: 2                                       |             |                 |          |   | SHEET NO. 29            |

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

**GPI** GREENMAN-PEDERSEN, INC. CONSULTING ENGINEERS  
 MONTGOMERY COUNTY DEPARTMENT OF PUBLIC WORKS

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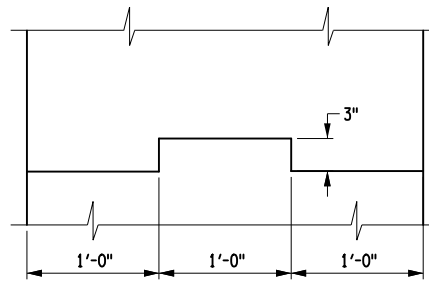
DESIGN SUPERVISOR: J. SIMKULET  
 JOB MANAGER: D. THOMAS  
 DESIGN: S. KERN  
 CHECK: B. WEAVER  
 DRAFTING: F. CUOCCIO  
 CHECK: S. KERN  
 PROJECT MANAGER: S. KERN



**WATERSTOP NOTES:**

- Holes must not be made in waterstop for any purpose except as required for stapling to forms.
- Type D waterstop shall be light gray in color.
- PVC used in waterstops shall conform to the requirements of N.Y.S. Standard Specifications Subsection 705-11.
- The cost of furnishing and placing waterstops shall be included in the unit prices bid for concrete, item 555.09.
- Field splices should be avoided if possible, however, heat welded butt splices will be permitted on long straight runs (generally in excess of 50 feet) at points approved by the engineer.
- Waterstop shall be shipped in straight sections having a minimum length of 10 feet unless shorter lengths are required.

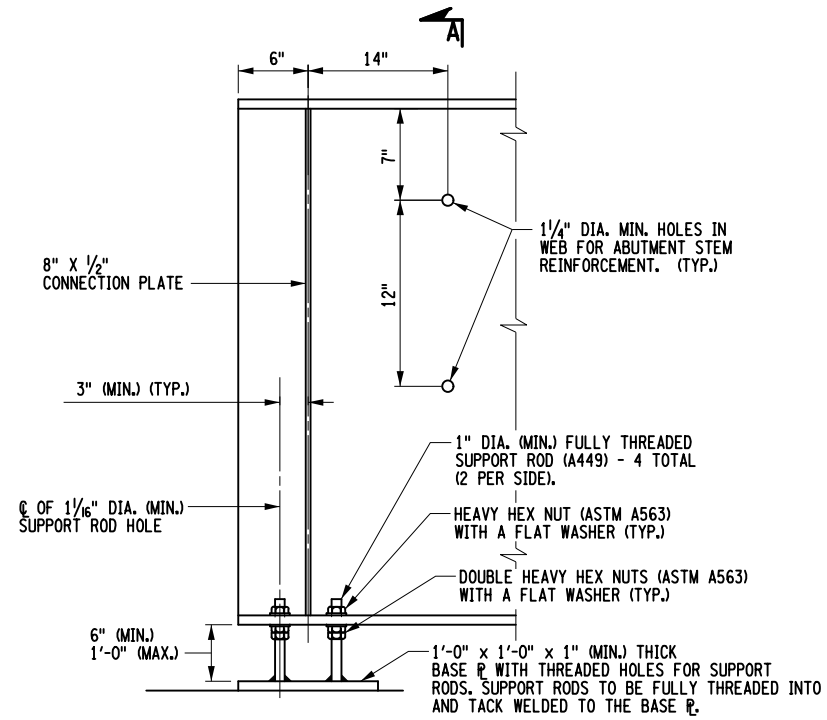
**TYPE D WATERSTOP**  
NOT TO SCALE



**KEYWAY NOTES:**

- Reinforcement not shown for clarity. Keyway to stop 1'-0" from support plate or construction joint.

**KEYWAY DETAIL**  
NOT TO SCALE



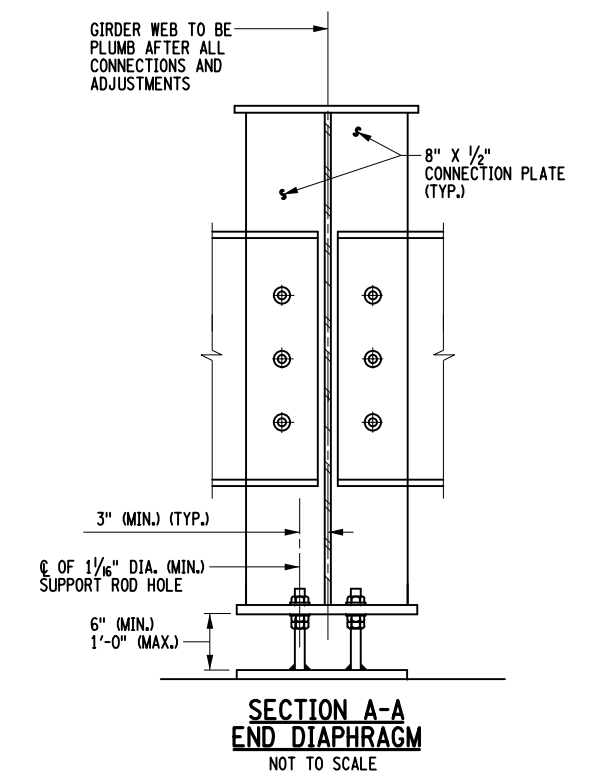
**TYPICAL GIRDER ELEVATION**  
NOT TO SCALE

**GIRDER SUPPORT NOTES:**


- Provide oversized holes in the bottom flange for support rods.
- Cost of temporary steel girder support is to be included in price bid for the girder, item 564.0501.

**INTEGRAL ABUTMENT CONSTRUCTION PROCEDURE FOR STEEL SUPERSTRUCTURE WITH TEMPORARY GIRDER SUPPORT**



- Drive piles and cut off at elevation 585.50.
- Place abutment stem concrete to bridge seat elevation.
- Backfill abutment stems to 6" below the bridge seat elevation. No backfill of the abutment stems allowed until the abutments have cured for 7 days.
- Place stone fill or slope protection.
- Erect girders and install all diaphragms. Girders shall be erected plumb and the diaphragms fabricated for the steel dead load position.
- Place abutment backwall concrete to top of girders.
- Backfill abutment backwalls. No backfilling of the abutment is allowed until backwalls have cured for 7 days.
- Place concrete for deck slab (contractor may elect to place concrete for deck slab prior to backfilling abutments. Deck slab shall not be placed until backwalls have cured for 7 days).
- Place concrete for approach slabs.



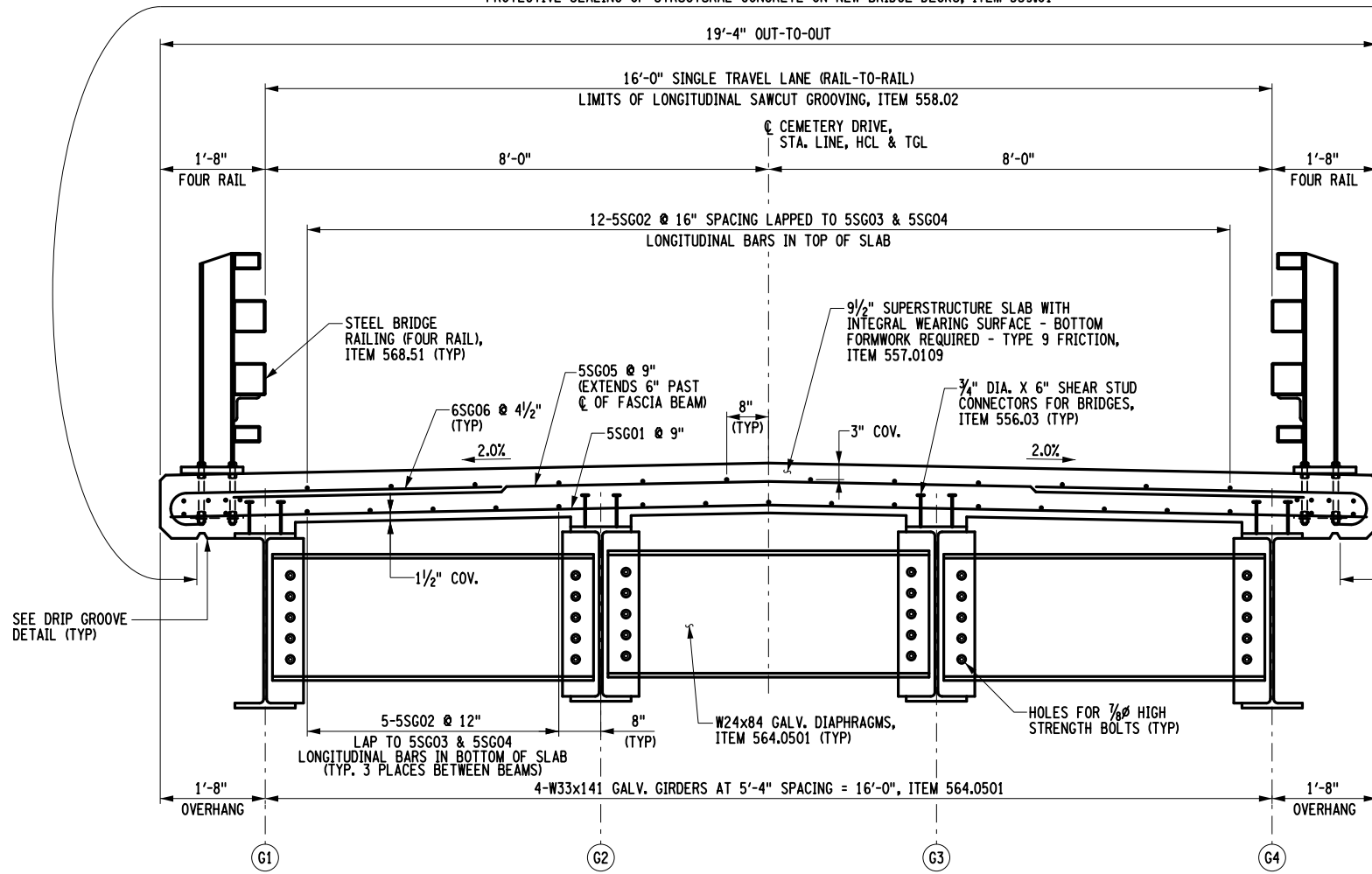
**SECTION A-A**  
**END DIAPHRAGM**  
NOT TO SCALE

AFFIX SEAL: GREENMAN - PEDERSEN, INC  
 ON: 

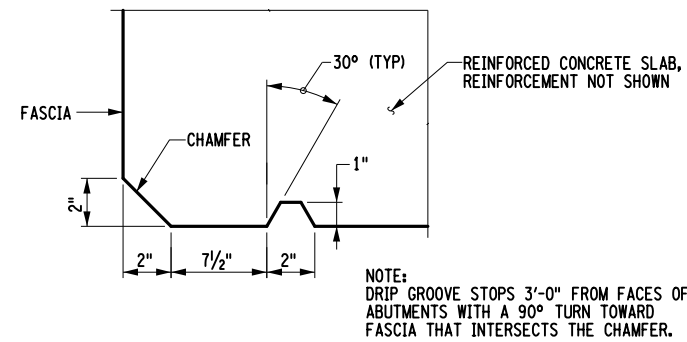
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|--|--|-------------|--------------------|----------|---|---|---|
| AS-BUILT REVISIONS<br>DESCRIPTION OF ALTERATIONS:  | CEMETERY DRIVE OVER THE                    | PIN 2754.54 | BRIDGES<br>3310200 | CULVERTS | ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED | CONTRACT NUMBER<br>D036212  |   |
|  | SOUTH CHUCTANUNDA CREEK BRIDGE REPLACEMENT |             |                    |          | MISCELLANEOUS ABUTMENT DETAILS              | DRAWING NO. ST-13<br>SHEET NO. 30   |   |
|  | TOWN OF FLORIDA                            |             |                    |          |   |   |   |
|  | COUNTY: MONTGOMERY                         | REGION: 2   |                    |          |   |   |   |
| IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION. |  |             |                    |          |   |  GREENMAN-PEDERSEN, INC.<br>CONSULTING ENGINEERS |  MONTGOMERY COUNTY<br>DEPARTMENT OF PUBLIC WORKS |

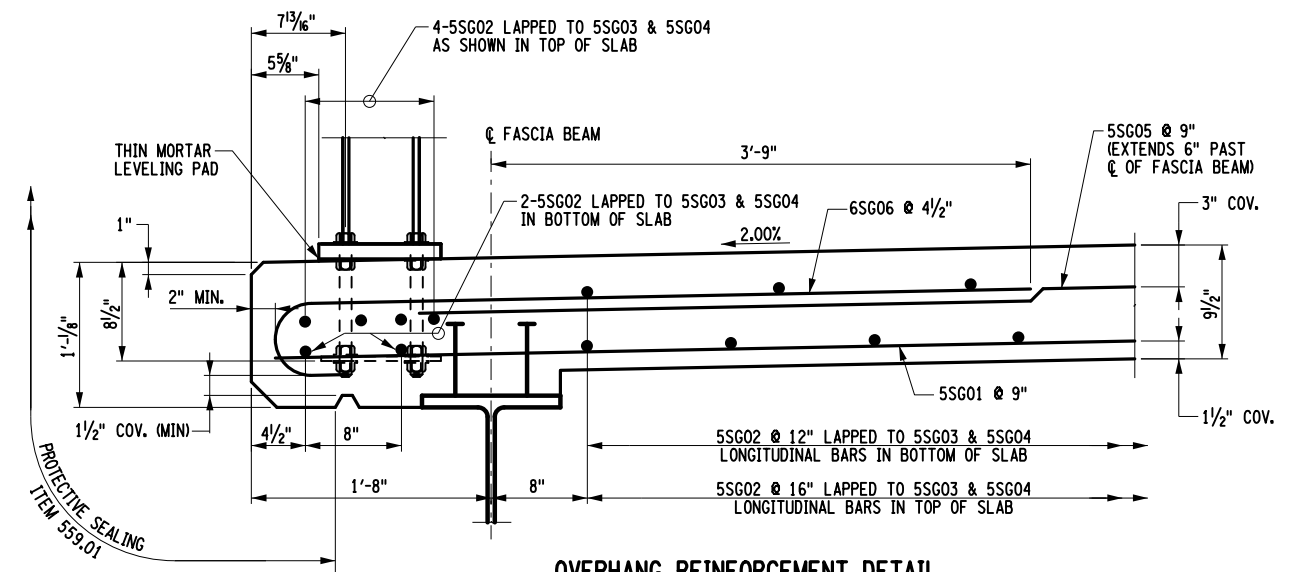
PROTECTIVE SEALING OF STRUCTURAL CONCRETE ON NEW BRIDGE DECKS, ITEM 559.01



**PROPOSED TRANSVERSE SECTION**  
 SCALE: 3/8"=1'-0"



**DRIP GROOVE DETAIL**  
 NOT TO SCALE



**OVERHANG REINFORCEMENT DETAIL**  
 SCALE: 3/4"=1'-0"

**NOTES:**  
 1. REINFORCEMENT IS TO BE GALVANIZED AND PAID UNDER ITEM 557.0109 - SUPERSTRUCTURE SLAB.

AFFIX SEAL: GREENMAN - PEDERSEN, INC ON: ALTERED BY: ON:



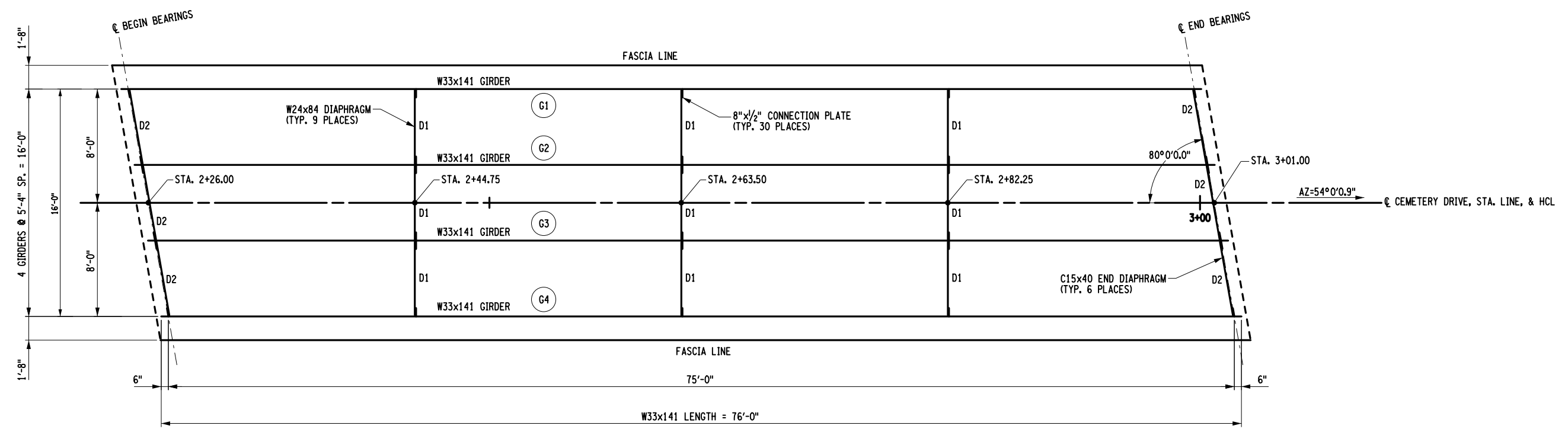
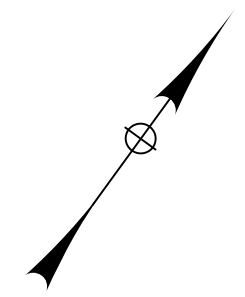
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|--|--|-------------|-----------------|----------|---|--|---|
| AS-BUILT REVISIONS<br>DESCRIPTION OF ALTERATIONS:  | CEMETERY DRIVE OVER THE                    | PIN 2754.54 | BRIDGES 3310200 | CULVERTS | ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED | CONTRACT NUMBER<br>D036212                                 |   |
|  | SOUTH CHUCTANUNDA CREEK BRIDGE REPLACEMENT |             |                 |          |   | DRAWING NO. ST-14<br>SHEET NO. 31                          |   |
|  | TOWN OF FLORIDA                            |             |                 |          | TRANSVERSE SECTION                          |  |   |
|  | COUNTY: MONTGOMERY                         | REGION: 2   |                 |          |   |  |   |
| IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION. |  |             |                 |          |   | <b>GPI</b> GREENMAN-PEDERSEN, INC.<br>CONSULTING ENGINEERS | MONTGOMERY COUNTY<br>DEPARTMENT OF PUBLIC WORKS |

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 DATE/TIME = 4/13/2022  
 USER = skern

PROJECT MANAGER S. KERN  
 CHECK S. KERN  
 DRAFTING F. CIUCCIO  
 CHECK B. WEAVER  
 DESIGN S. KERN  
 JOB MANAGER D. THOMAS  
 DESIGN SUPERVISOR J. SIMKULET

PROJECT MANAGER S. KERN  
 CHECK S. KERN  
 DRAFTING F. CIUCCIO  
 CHECK B. WEAVER  
 DESIGN S. KERN  
 JOB MANAGER D. THOMAS  
 DESIGN SUPERVISOR J. SIMKULET

FILE NAME = ... \CADD\275454\_CPB\_FRM\_PLN.dgn  
 DATE/TIME = 4/13/2022  
 USER = skern



**FRAMING PLAN**  
 SCALE: 1/8"=1'-0"

- NOTES:**
- ALL STEEL SHOWN IS TO BE PAID UNDER ITEM 564.0501 AND SHALL BE GALVANIZED UNDER ITEM 564.20010008.

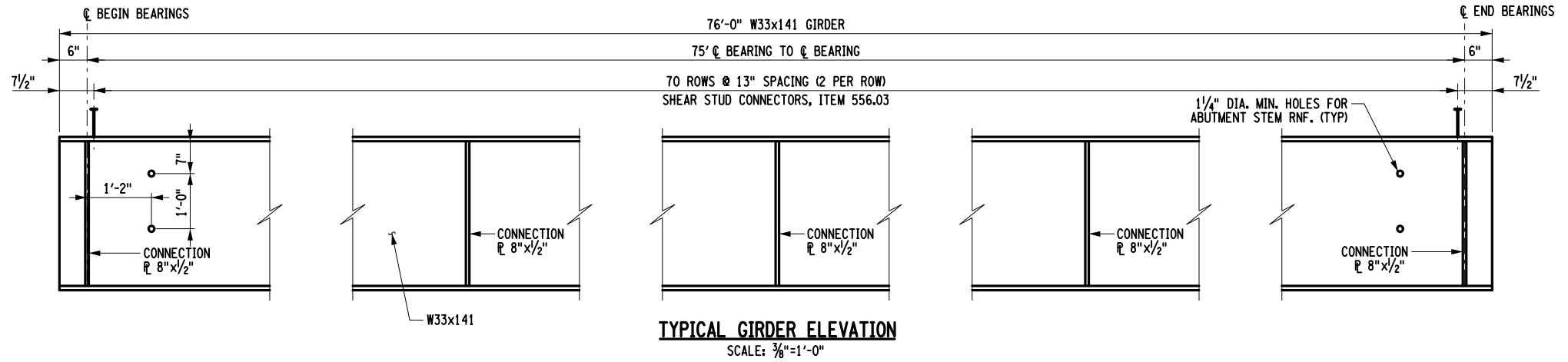
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| AFFIX SEAL: GREENMAN - PEDERSEN, INC<br>ON: | ALTERED BY:<br>ON: |
|   |                    |

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|---|--|-------------|--------------------|----------|---|-----------------------------------|
| AS-BUILT REVISIONS<br>DESCRIPTION OF ALTERATIONS: | CEMETERY DRIVE OVER THE                    | PIN 2754.54 | BRIDGES<br>3310200 | CULVERTS | ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED | CONTRACT NUMBER<br>D036212        |
|   | SOUTH CHUCTANUNDA CREEK BRIDGE REPLACEMENT |             |                    |          |   | DRAWING NO. ST-15<br>SHEET NO. 32 |
|   | TOWN OF FLORIDA                            |             |                    |          | FRAMING PLAN                                |                                   |
|   | COUNTY: MONTGOMERY                         | REGION: 2   |                    |          |   |                                   |

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

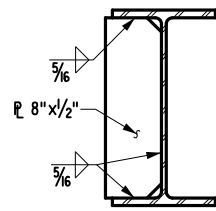
| DESIGN LOAD TABLE |              | BEAM LOAD (K/FT) |       |      |
|-------------------|--------------|------------------|-------|------|
| UNIT              |              | B1               | B2-B3 | B4   |
| D.L.              | SLAB         | 0.51             | 0.64  | 0.51 |
|                   | HAUNCH       | 0.05             | 0.02  | 0.05 |
|                   | GIRDER       | 0.01             | 0.02  | 0.01 |
|                   | DIAPHRAGMS   | 0.14             | 0.14  | 0.14 |
|                   | S.I.P. FORMS | 0.01             | 0.02  | 0.01 |
| TOTAL             |              | 0.72             | 0.84  | 0.72 |
| S.D.L.            | RAILING      | 0.05             | 0.05  | 0.05 |
|                   | FUTURE W.S.  | 0.10             | 0.10  | 0.10 |
| TOTAL             |              | 0.15             | 0.15  | 0.15 |

LIVE LOAD: HL-93

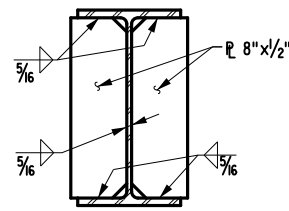


**NOTES:**

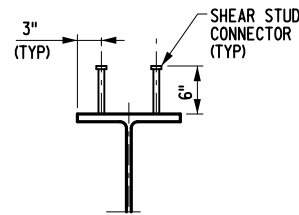
- UNLESS SHOWN IN THESE DETAILS, THERE SHALL BE NO WELDING WITHIN THE TENSION ZONES. THE ATTACHMENT OF FORMING DEVICES OR OTHER CONSTRUCTION AIDS BY WELDING WITHIN THE TENSION AREA (ENTIRE BOTTOM PLATE) IS PROHIBITED.
- THERE SHALL BE NO WELDING TO GALVANIZED OR METALIZED GIRDERS FOR THE ATTACHMENT OF FORMING DEVICES.
- THE ENDS OF ALL BEAMS SHALL BE VERTICAL. ALL CONNECTION PLATES AND INTERMEDIATE STIFFENERS MAY BE PERPENDICULAR TO THE TOP FLANGES.
- CONNECTIONS SHALL BE MADE ACCORDING TO THE NEW YORK STATE STEEL CONSTRUCTION MANUAL.
- UNLESS OTHERWISE INDICATED, BOLTED CONNECTIONS SHALL BE MADE WITH 7/8" DIA. ASTM F3125, GRADE A325 HIGH-STRENGTH BOLTS. THE COST OF FURNISHING AND PLACING THE H.S. BOLTS SHALL BE PAID FOR UNDER THE STRUCTURAL STEEL ITEM 564.0501
- THE CONTRACTOR MAY PLACE DIAPHRAGMS ON EITHER SIDE OF THE BEARING STIFFENERS OR CONNECTION PLATES AS NECESSARY FOR CORRECT ALIGNMENT, PROVIDED THERE WILL BE NO INTERFERENCE WITH OTHER STRUCTURAL DETAILS.
- TAPERED OR FLAT SHIM PLATES MAY BE USED IN THE CONNECTION BETWEEN SKEWED DIAPHRAGMS AND THE BEARING STIFFENERS, STIFFENER CONNECTION PLATES OR GUSSET PLATES. VARIABLE THICKNESSES OF SHIM PLATES MAY BE USED. THE MINIMUM THICKNESS OF SHIM PLATE SHALL BE 1/8" WITH A MAXIMUM NUMBER OF THREE SHIM PLATES PERMITTED AT ANY CONNECTION. THE TOTAL THICKNESS OF ALL SHIM PLATES USED AT ANY CONNECTION SHALL NOT EXCEED 1". SHIM PLATES SHALL HAVE THE DIMENSIONS OF THE FAYING SURFACE. THE SHIM MATERIAL SHALL CONFORM TO ASTM DESIGNATION A709(W) FOR WEATHERING STEEL APPLICATIONS. NO ADDITIONAL PAYMENT WILL BE MADE FOR FURNISHING AND PLACING THE SHIM PLATES.
- DIAPHRAGM MEMBERS SHALL BE BLOCKED AS SHOWN, WITH THEIR FLANGE CUT BACK ON ONE SIDE, AND CHIPPED OR GROUND FLUSH. IN LIEU OF BLOCKING THE DIAPHRAGM MEMBER, THE FABRICATOR SHALL HAVE THE OPTION OF COPING THE FLANGE.



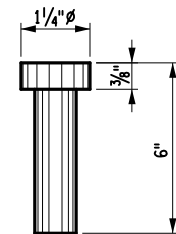
**FASCIA GIRDER CONNECTION P**  
SCALE: NONE



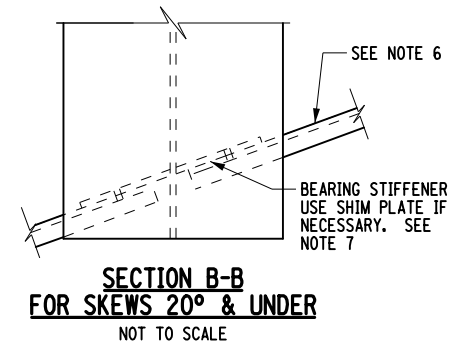
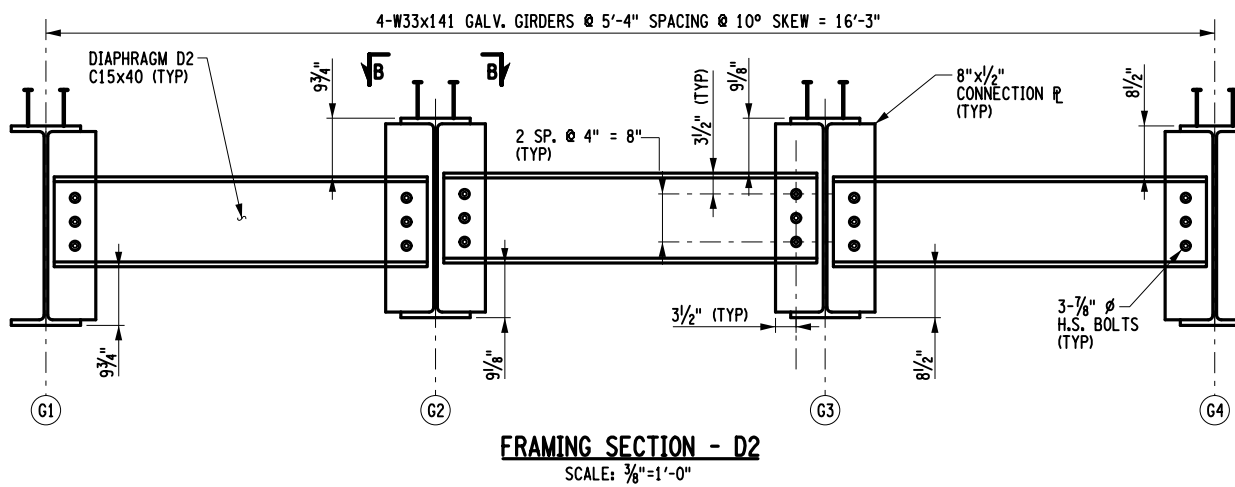
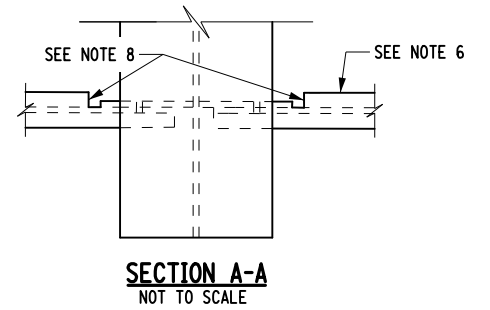
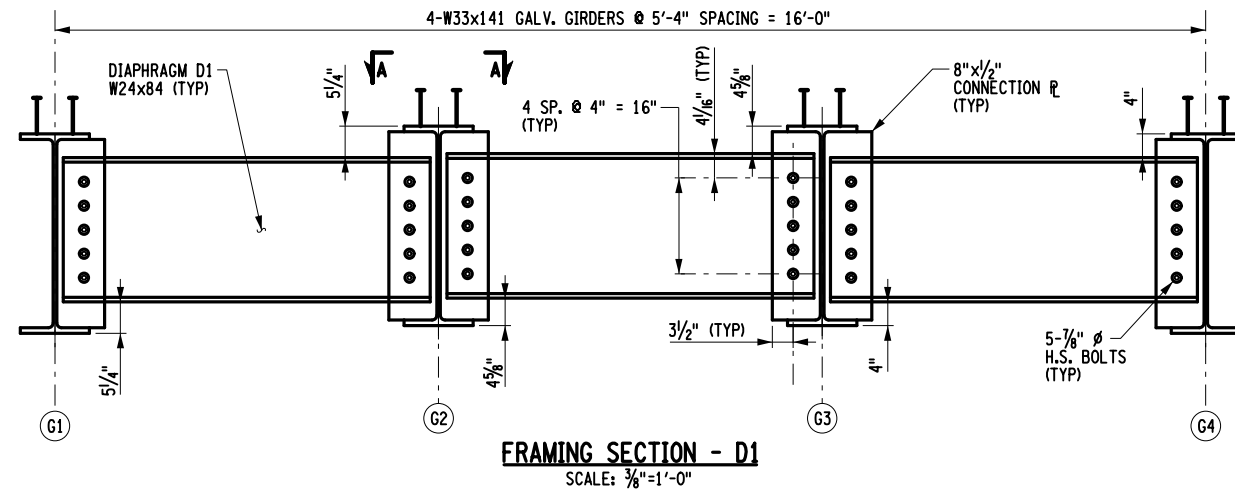
**INTERIOR GIRDER CONNECTION P**  
SCALE: NONE



**SHEAR STUD CONNECTOR DETAIL**  
SCALE: NONE



**STUD DETAIL**  
SCALE: NONE



AFFIX SEAL: GREENMAN - PEDERSEN, INC ON: ALTERED BY: ON:



|  |  |             |                 |          |   |                         |
|--|--|-------------|-----------------|----------|---|-------------------------|
| AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS: | CEMETERY DRIVE OVER THE SOUTH CHUCTANUNDA CREEK BRIDGE REPLACEMENT | PIN 2754.54 | BRIDGES 3310200 | CULVERTS | ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED | CONTRACT NUMBER D036212 |
|  | TOWN OF FLORIDA  |             |                 |          | GIRDER ELEVATION AND DETAILS                | DRAWING NO. ST-16       |
|  | COUNTY: MONTGOMERY REGION: 2                                       |             |                 |          |   | SHEET NO. 33            |

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

**GPI** GREENMAN-PEDERSEN, INC. CONSULTING ENGINEERS

MONTGOMERY COUNTY DEPARTMENT OF PUBLIC WORKS

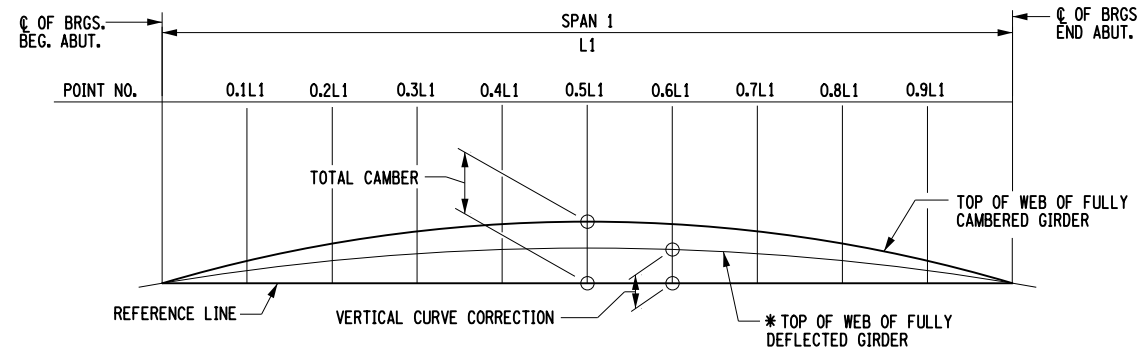
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USER = skern

PROJECT MANAGER S. KERN  
CHECK S. KERN  
DRAFTING F. CUOCCIO  
CHECK B. WEAVER  
DESIGN S. KERN  
JOB MANAGER D. THOMAS  
DESIGN SUPERVISOR J. SIMKULET



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 USER = skern

DESIGN SUPERVISOR J. SIMKULET  
 JOB MANAGER D. THOMAS  
 DESIGN S. KERN  
 CHECK B. WEAVER  
 DRAFTING F. CIUCCIO  
 CHECK S. KERN  
 PROJECT MANAGER S. KERN

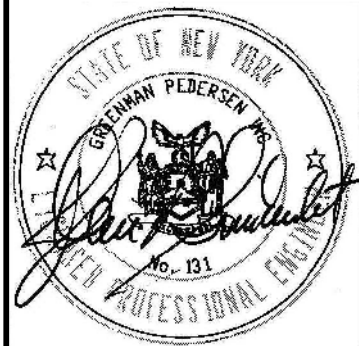


**CAMBER DIAGRAM - SIMPLE SPANS**  
 SCALE: NONE

| CAMBER TABLE |                            | CL. OF BRGS. BEGIN. ABUT. | 0.1 x SPAN | 0.2 x SPAN | 0.3 x SPAN | 0.4 x SPAN | 0.5 x SPAN | 0.6 x SPAN | 0.7 x SPAN | 0.8 x SPAN | 0.9 x SPAN | CL. OF BRGS. END ABUT. |
|--------------|----------------------------|---------------------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------------------|
| GIRDER 1     | I STEEL D.L. (ft)          | 0.000                     | 0.013      | 0.024      | 0.033      | 0.039      | 0.041      | 0.039      | 0.033      | 0.024      | 0.013      | 0.000                  |
|              | II CONCRETE D.L. (ft)      | 0.000                     | 0.049      | 0.093      | 0.127      | 0.148      | 0.156      | 0.148      | 0.127      | 0.093      | 0.049      | 0.000                  |
|              | III SUPERIMPOSED D.L. (ft) | 0.000                     | 0.007      | 0.014      | 0.019      | 0.022      | 0.023      | 0.022      | 0.019      | 0.014      | 0.007      | 0.000                  |
|              | IV VERTICAL CURVE (ft)     | 0.000                     | 0.000      | 0.000      | 0.000      | 0.000      | 0.000      | 0.000      | 0.000      | 0.000      | 0.000      | 0.000                  |
|              | TOTAL = I+II+III+IV (ft)   | 0.000                     | 0.069      | 0.130      | 0.178      | 0.209      | 0.220      | 0.209      | 0.178      | 0.130      | 0.069      | 0.000                  |
| GIRDER 2     | I STEEL D.L. (ft)          | 0.000                     | 0.013      | 0.024      | 0.033      | 0.039      | 0.041      | 0.039      | 0.033      | 0.024      | 0.013      | 0.000                  |
|              | II CONCRETE D.L. (ft)      | 0.000                     | 0.060      | 0.113      | 0.154      | 0.181      | 0.190      | 0.181      | 0.154      | 0.113      | 0.060      | 0.000                  |
|              | III SUPERIMPOSED D.L. (ft) | 0.000                     | 0.007      | 0.013      | 0.017      | 0.020      | 0.021      | 0.020      | 0.017      | 0.013      | 0.007      | 0.000                  |
|              | IV VERTICAL CURVE (ft)     | 0.000                     | 0.000      | 0.000      | 0.000      | 0.000      | 0.000      | 0.000      | 0.000      | 0.000      | 0.000      | 0.000                  |
|              | TOTAL = I+II+III+IV (ft)   | 0.000                     | 0.079      | 0.150      | 0.205      | 0.240      | 0.252      | 0.240      | 0.205      | 0.150      | 0.079      | 0.000                  |
| GIRDER 3     | I STEEL D.L. (ft)          | 0.000                     | 0.013      | 0.024      | 0.033      | 0.039      | 0.041      | 0.039      | 0.033      | 0.024      | 0.013      | 0.000                  |
|              | II CONCRETE D.L. (ft)      | 0.000                     | 0.060      | 0.113      | 0.154      | 0.181      | 0.190      | 0.181      | 0.154      | 0.113      | 0.060      | 0.000                  |
|              | III SUPERIMPOSED D.L. (ft) | 0.000                     | 0.007      | 0.013      | 0.017      | 0.020      | 0.021      | 0.020      | 0.017      | 0.013      | 0.007      | 0.000                  |
|              | IV VERTICAL CURVE (ft)     | 0.000                     | 0.000      | 0.000      | 0.000      | 0.000      | 0.000      | 0.000      | 0.000      | 0.000      | 0.000      | 0.000                  |
|              | TOTAL = I+II+III+IV (ft)   | 0.000                     | 0.079      | 0.150      | 0.205      | 0.240      | 0.252      | 0.240      | 0.205      | 0.150      | 0.079      | 0.000                  |
| GIRDER 4     | I STEEL D.L. (ft)          | 0.000                     | 0.013      | 0.024      | 0.033      | 0.039      | 0.041      | 0.039      | 0.033      | 0.024      | 0.013      | 0.000                  |
|              | II CONCRETE D.L. (ft)      | 0.000                     | 0.049      | 0.093      | 0.127      | 0.148      | 0.156      | 0.148      | 0.127      | 0.093      | 0.049      | 0.000                  |
|              | III SUPERIMPOSED D.L. (ft) | 0.000                     | 0.007      | 0.014      | 0.019      | 0.022      | 0.023      | 0.022      | 0.019      | 0.014      | 0.007      | 0.000                  |
|              | IV VERTICAL CURVE (ft)     | 0.000                     | 0.000      | 0.000      | 0.000      | 0.000      | 0.000      | 0.000      | 0.000      | 0.000      | 0.000      | 0.000                  |
|              | TOTAL = I+II+III+IV (ft)   | 0.000                     | 0.069      | 0.130      | 0.178      | 0.209      | 0.220      | 0.209      | 0.178      | 0.130      | 0.069      | 0.000                  |

| MOMENT & SHEAR TABLE |               |        | CL. OF BRGS. BEGIN. ABUT. | 0.1 x SPAN | 0.2 x SPAN | 0.3 x SPAN | 0.4 x SPAN | 0.5 x SPAN | 0.6 x SPAN | 0.7 x SPAN | 0.8 x SPAN | 0.9 x SPAN | CL. OF BRGS. END ABUT. |       |
|----------------------|---------------|--------|---------------------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------------------|-------|
| GIRDER 1             | D.L.          | MOMENT | 0.0                       | 181.2      | 322.2      | 422.9      | 483.3      | 503.4      | 483.3      | 422.9      | 322.2      | 181.2      | 0.0                    |       |
|                      |               | SHEAR  | 26.8                      | 21.5       | 16.1       | 10.7       | 5.4        | 0.0        | -5.4       | -10.7      | -16.1      | -21.5      | -26.8                  |       |
|                      | S.D.L.        | MOMENT | 0.0                       | 38.0       | 67.5       | 88.6       | 101.2      | 105.5      | 101.2      | 88.6       | 67.5       | 38.0       | 0.0                    |       |
|                      |               | SHEAR  | 5.6                       | 4.5        | 3.4        | 2.2        | 1.1        | 0.0        | -1.1       | -2.2       | -3.4       | -4.5       | -5.6                   |       |
|                      | HL-93 (+)     | MOMENT | 0.0                       | 269.6      | 471.9      | 606.8      | 685.5      | 702.4      | 685.5      | 606.8      | 471.9      | 269.6      | 0.0                    |       |
|                      |               | SHEAR  | 40.4                      | 35.1       | 30.0       | 25.1       | 20.3       | 15.7       | 11.3       | 7.6        | 4.7        | 1.9        | 0.0                    |       |
|                      | HL-93 (-)     | MOMENT | 0.0                       | -1.9       | -4.7       | -7.6       | -11.3      | -15.7      | -20.3      | -25.1      | -30.0      | -35.1      | -40.4                  |       |
|                      |               | SHEAR  | 0.0                       | -1.9       | -4.7       | -7.6       | -11.3      | -15.7      | -20.3      | -25.1      | -30.0      | -35.1      | -40.4                  |       |
|                      | GIRDERS 2 - 3 | D.L.   | MOMENT                    | 0.0        | 212.3      | 377.4      | 495.4      | 566.1      | 589.7      | 566.1      | 495.4      | 377.4      | 212.3                  | 0.0   |
|                      |               |        | SHEAR                     | 31.5       | 25.2       | 18.9       | 12.6       | 6.3        | 0.0        | -6.3       | -12.6      | -18.9      | -25.2                  | -31.5 |
|                      |               | S.D.L. | MOMENT                    | 0.0        | 38.0       | 67.5       | 88.6       | 101.2      | 105.5      | 101.2      | 88.6       | 67.5       | 38.0                   | 0.0   |
|                      |               |        | SHEAR                     | 5.6        | 4.5        | 3.4        | 2.3        | 1.1        | 0.0        | -1.1       | -2.3       | -3.4       | -4.5                   | -5.6  |
| HL-93 (+)            |               | MOMENT | 0.0                       | 251.9      | 440.9      | 566.9      | 640.4      | 656.2      | 640.4      | 566.9      | 440.9      | 251.9      | 0.0                    |       |
|                      |               | SHEAR  | 61.8                      | 53.7       | 45.9       | 38.3       | 31.1       | 24.1       | 17.3       | 11.7       | 7.2        | 2.9        | 0.0                    |       |
| HL-93 (-)            |               | MOMENT | 0.0                       | -2.9       | -7.2       | -11.7      | -17.3      | -24.1      | -31.1      | -38.3      | -45.9      | -53.7      | -61.8                  |       |
|                      |               | SHEAR  | 0.0                       | -2.9       | -7.2       | -11.7      | -17.3      | -24.1      | -31.1      | -38.3      | -45.9      | -53.7      | -61.8                  |       |
| GIRDER 4             |               | D.L.   | MOMENT                    | 0.0        | 181.2      | 322.2      | 422.9      | 483.3      | 503.4      | 483.3      | 422.9      | 322.2      | 181.2                  | 0.0   |
|                      |               |        | SHEAR                     | 26.8       | 21.5       | 16.1       | 10.7       | 5.4        | 0.0        | -5.4       | -10.7      | -16.1      | -21.5                  | -26.8 |
|                      |               | S.D.L. | MOMENT                    | 0.0        | 38.0       | 67.5       | 88.6       | 101.2      | 105.5      | 101.2      | 88.6       | 67.5       | 38.0                   | 0.0   |
|                      |               |        | SHEAR                     | 5.6        | 4.5        | 3.4        | 2.2        | 1.1        | 0.0        | -1.1       | -2.2       | -3.4       | -4.5                   | -5.6  |
|                      | HL-93 (+)     | MOMENT | 0.0                       | 269.6      | 471.9      | 606.8      | 685.5      | 702.4      | 685.5      | 606.8      | 471.9      | 269.6      | 0.0                    |       |
|                      |               | SHEAR  | 40.4                      | 35.1       | 30.0       | 25.1       | 20.3       | 15.7       | 11.3       | 7.6        | 4.7        | 1.9        | 0.0                    |       |
|                      | HL-93 (-)     | MOMENT | 0.0                       | -1.9       | -4.7       | -7.6       | -11.3      | -15.7      | -20.3      | -25.1      | -30.0      | -35.1      | -40.4                  |       |
|                      |               | SHEAR  | 0.0                       | -1.9       | -4.7       | -7.6       | -11.3      | -15.7      | -20.3      | -25.1      | -30.0      | -35.1      | -40.4                  |       |

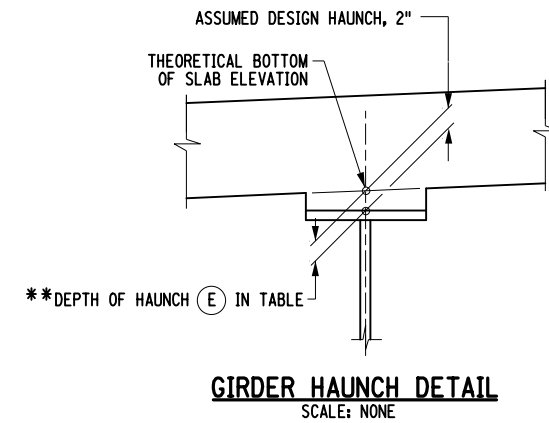
AFFIX SEAL: GREENMAN - PEDERSEN, INC ON: \_\_\_\_\_  
 ALTERED BY: \_\_\_\_\_ ON: \_\_\_\_\_




|   |   |             |                    |          |  |   |
|---|---|-------------|--------------------|----------|--|---|
| AS-BUILT REVISIONS<br>DESCRIPTION OF ALTERATIONS: | CEMETERY DRIVE OVER THE<br>SOUTH CHUCTANUNDA CREEK BRIDGE REPLACEMENT | PIN 2754.54 | BRIDGES<br>3310200 | CULVERTS | ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED                | CONTRACT NUMBER<br>D036212                      |
|   | TOWN OF FLORIDA   |             |                    |          | DESIGN TABLES<br>(1 OF 2)                                  | DRAWING NO. ST-17<br>SHEET NO. 34               |
|   | COUNTY: MONTGOMERY REGION: 2  |             |                    |          | <b>GPI</b> GREENMAN-PEDERSEN, INC.<br>CONSULTING ENGINEERS | MONTGOMERY COUNTY<br>DEPARTMENT OF PUBLIC WORKS |

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

| HAUNCH TABLE |                                       | CL. OF BRGS. BEGIN. ABUT. | 0.1x SPAN | 0.2x SPAN | 0.3x SPAN | 0.4x SPAN | 0.5x SPAN | 0.6x SPAN | 0.7x SPAN | 0.8x SPAN | 0.9x SPAN | CL. OF BRGS. END ABUT. |
|--------------|---------------------------------------|---------------------------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|------------------------|
| GIRDER 1     | A REQ'D BOTTOM OF SLAB ELEVATION      | 594.610                   | 594.586   | 594.561   | 594.536   | 594.511   | 594.487   | 594.462   | 594.437   | 594.412   | 594.388   | 594.363                |
|              | B TOP OF STEEL EL. (FIELD MEASURE)    |                           |           |           |           |           |           |           |           |           |           |                        |
|              | C = A - B                             |                           |           |           |           |           |           |           |           |           |           |                        |
|              | D CONCRETE + S.D.L. DEFLECTION        | 0.000                     | 0.056     | 0.106     | 0.145     | 0.170     | 0.179     | 0.170     | 0.145     | 0.106     | 0.056     | 0.000                  |
|              | E DEPTH OF HAUNCH REQ'D = C + D (ft.) |                           |           |           |           |           |           |           |           |           |           |                        |
| GIRDER 2     | A REQ'D BOTTOM OF SLAB ELEVATION      | 594.714                   | 594.689   | 594.664   | 594.640   | 594.615   | 594.590   | 594.565   | 594.541   | 594.516   | 594.491   | 594.466                |
|              | B TOP OF STEEL EL. (FIELD MEASURE)    |                           |           |           |           |           |           |           |           |           |           |                        |
|              | C = A - B                             |                           |           |           |           |           |           |           |           |           |           |                        |
|              | D CONCRETE + S.D.L. DEFLECTION        | 0.000                     | 0.066     | 0.125     | 0.172     | 0.201     | 0.211     | 0.201     | 0.172     | 0.125     | 0.066     | 0.000                  |
|              | E DEPTH OF HAUNCH REQ'D = C + D (ft.) |                           |           |           |           |           |           |           |           |           |           |                        |
| GIRDER 3     | A REQ'D BOTTOM OF SLAB ELEVATION      | 594.711                   | 594.686   | 594.661   | 594.637   | 594.612   | 594.587   | 594.562   | 594.538   | 594.513   | 594.488   | 594.463                |
|              | B TOP OF STEEL EL. (FIELD MEASURE)    |                           |           |           |           |           |           |           |           |           |           |                        |
|              | C = A - B                             |                           |           |           |           |           |           |           |           |           |           |                        |
|              | D CONCRETE + S.D.L. DEFLECTION        | 0.000                     | 0.066     | 0.125     | 0.172     | 0.201     | 0.211     | 0.201     | 0.172     | 0.125     | 0.066     | 0.000                  |
|              | E DEPTH OF HAUNCH REQ'D = C + D (ft.) |                           |           |           |           |           |           |           |           |           |           |                        |
| GIRDER 4     | A REQ'D BOTTOM OF SLAB ELEVATION      | 594.601                   | 594.576   | 594.551   | 594.527   | 594.502   | 594.477   | 594.452   | 594.428   | 594.403   | 594.378   | 594.353                |
|              | B TOP OF STEEL EL. (FIELD MEASURE)    |                           |           |           |           |           |           |           |           |           |           |                        |
|              | C = A - B                             |                           |           |           |           |           |           |           |           |           |           |                        |
|              | D CONCRETE + S.D.L. DEFLECTION        | 0.000                     | 0.056     | 0.106     | 0.145     | 0.170     | 0.179     | 0.170     | 0.145     | 0.106     | 0.056     | 0.000                  |
|              | E DEPTH OF HAUNCH REQ'D = C + D (ft.) |                           |           |           |           |           |           |           |           |           |           |                        |



**NOTES:**  
 1. THE CONTRACTOR SHALL PROVIDE THE ENGINEER WITH THE COMPLETED HAUNCH TABLE PRIOR TO SETTING THE BOTTOM FORM WORK OF THE DECK.

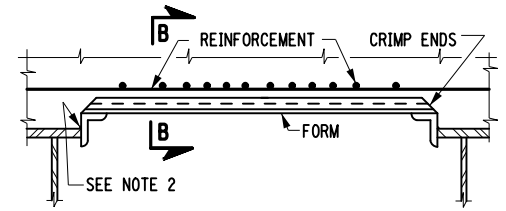
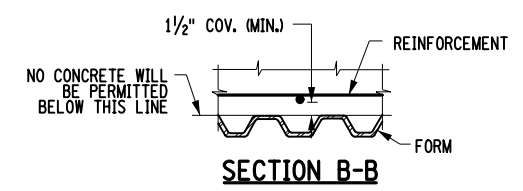
|   |                    |
|---|--------------------|
| AFFIX SEAL: GREENMAN - PEDERSEN, INC<br>ON:   | ALTERED BY:<br>ON: |
|  |                    |

|   |   |             |                    |          |  |   |
|---|---|-------------|--------------------|----------|--|---|
| AS-BUILT REVISIONS<br>DESCRIPTION OF ALTERATIONS: | CEMETERY DRIVE OVER THE<br>SOUTH CHUCTANUNDA CREEK BRIDGE REPLACEMENT | PIN 2754.54 | BRIDGES<br>3310200 | CULVERTS | ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED                | CONTRACT NUMBER<br>D036212                      |
|   | TOWN OF FLORIDA   |             |                    |          | DESIGN TABLES<br>(2 OF 2)                                  | DRAWING NO. ST-18<br>SHEET NO. 35               |
|   | COUNTY: MONTGOMERY  | REGION: 2   |                    |          | <b>GPI</b> GREENMAN-PEDERSEN, INC.<br>CONSULTING ENGINEERS | MONTGOMERY COUNTY<br>DEPARTMENT OF PUBLIC WORKS |

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

FILE NAME = ...CADD\275454\_CPB\_SLB\_DCK.dgn  
 DATE/TIME = 4/13/2022  
 USER = skern

DESIGN SUPERVISOR J. SIMKULET  
 JOB MANAGER D. THOMAS  
 DESIGN S. KERN  
 CHECK B. WEAVER  
 DRAFTING F. CUOCCIO  
 CHECK S. KERN  
 PROJECT MANAGER S. KERN



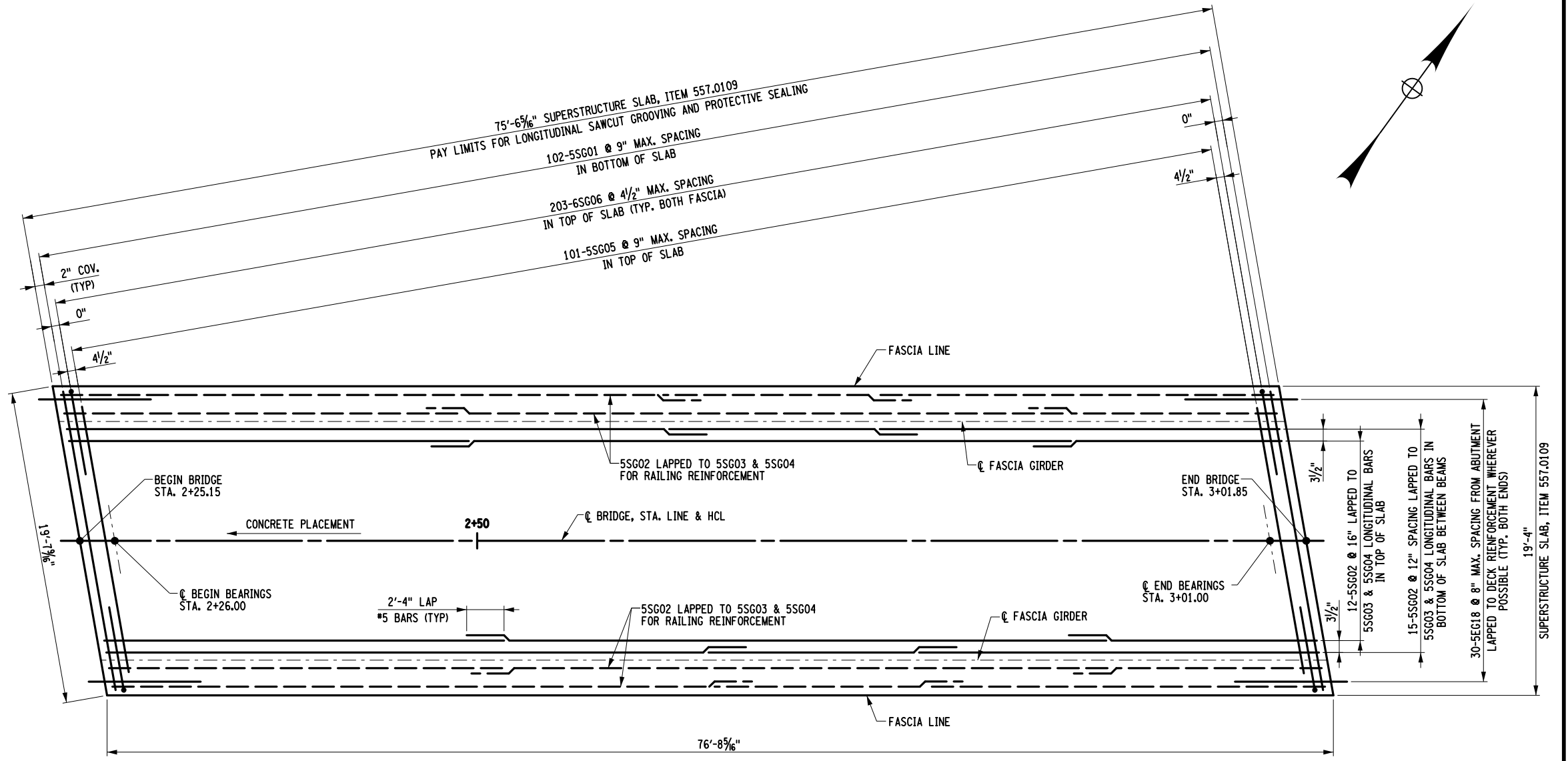
**FORM UNIT NOTES:**

1. THE COST OF THE FORMING SYSTEMS SHOWN ON THIS DRAWING SHALL BE INCLUDED IN THE UNIT PRICE BID FOR SUPERSTRUCTURE SLAB CONCRETE ITEM 557.0109.
2. THERE SHALL BE NO WELDING TO GALVANIZED OR METALIZED GIRDER SURFACES FOR THE ATTACHMENT OF FORMING DEVICES.
3. THE SUPPORT ANGLES SHALL BE GALVANIZED IN ACCORDANCE WITH MATERIAL SPECIFICATION 719-01.

**PERMANENT CORRUGATED METAL FORM DETAIL**  
 SCALE: NONE

**DECK PLACEMENT NOTES:**

1. CONCRETE PLACEMENT AND FINISHING OPERATIONS SHALL BE PERFORMED AS RAPIDLY AS POSSIBLE. THE ENGINEER MAY ORDER THE CONTRACTOR TO STOP PLACEMENT OPERATIONS AT ANY TIME IF, IN THE ENGINEER'S OPINION, CONCRETE PLACED DURING THE PLACEMENT HAS STARTED TO SET, OR IS ABOUT TO SET, AND FURTHER PLACEMENT OF CONCRETE WILL CAUSE DEFLECTION CRACKING.
2. LONGITUDINAL CONSTRUCTION JOINTS WILL NOT BE PERMITTED.
3. THE CONTRACTOR SHALL OPERATE FINISHING MACHINE(S) AS CLOSE TO THE SKEW ANGLE AS PRACTICABLE FOR SKEW ANGLES BETWEEN 0° AND 35°.
4. THE CONTRACTOR SHALL PLACE WET BURLAP CURING BLANKETS ON THE CONCRETE DECK WITHIN 30 MINUTES OF THE CONCRETE BEING DEPOSITED INTO THE FORMS OR 5 MINUTES AFTER FINISHING, WHICHEVER COMES FIRST. THE PLACEMENT OF THE TURF DRAG TEXTURE SHALL NOT INTERFERE WITH THESE REQUIREMENTS.
5. IF THE CONTRACTOR'S DECK PLACEMENT OPERATION IS STOPPED PRIOR TO COMPLETION, WHETHER BY THE CONTRACTOR'S DECISION OR BY DIRECTION OF THE ENGINEER, THE CONTRACTOR SHALL PROVIDE A FINISHED DECK GRADE WHICH MATCHES THE PLANNED PROFILE. ANY SUBSEQUENT REVISIONS TO DECK FORMS MADE NECESSARY BY SUCH ACTION SHALL BE AT NO COST TO THE COUNTY.



**SUPERSTRUCTURE SLAB REINFORCEMENT PLAN**  
 SCALE: 1/8"=1'-0"

| SUPERSTRUCTURE SLAB TABLE                 |  |   |
|---|--|---|
| SUPERSTRUCTURE SLAB (SY)<br>ITEM 557.0109 | LONGITUDINAL SAWCUT GROOVING (SY)<br>ITEM 558.02 | PROTECTIVE SEALING OF NEW DECKS (SF)<br>ITEM 559.01 |
| 165                                       | 137  | 1,758   |

NOTE: FOR TRANSVERSE LIMITS OF LONGITUDINAL SAWCUT GROOVING AND PROTECTIVE SEALING ITEMS, REFER TO THE TRANSVERSE SECTION DRAWING.

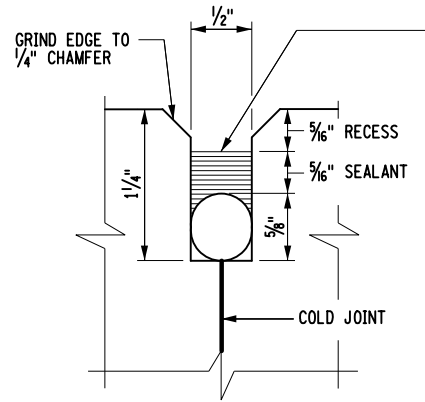
AFFIX SEAL: GREENMAN - PEDERSEN, INC  
 ON:

ALTERED BY:  
 ON:

|  |  |             |                 |          |  |  |
|--|--|-------------|-----------------|----------|--|--|
| AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:   | CEMETERY DRIVE OVER THE SOUTH CHUCTANUNDA CREEK BRIDGE REPLACEMENT | PIN 2754.54 | BRIDGES 3310200 | CULVERTS | ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED  | CONTRACT NUMBER D036212                      |
|  | TOWN OF FLORIDA  |             |                 |          | SUPERSTRUCTURE SLAB REINFORCEMENT PLAN       | DRAWING NO. ST-19                            |
|  | COUNTY: MONTGOMERY REGION: 2                                       |             |                 |          |  | SHEET NO. 36                                 |
| IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION. |  |             |                 |          | GREENMAN-PEDERSEN, INC. CONSULTING ENGINEERS | MONTGOMERY COUNTY DEPARTMENT OF PUBLIC WORKS |

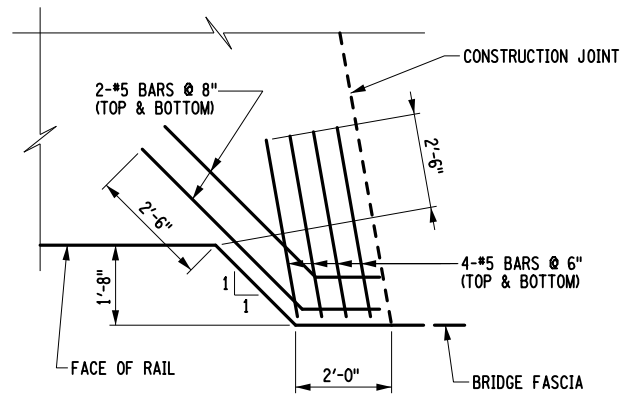
PROJECT MANAGER S. KERN  
 CHECK S. KERN  
 DRAFTING F. CUOCCIO  
 CHECK B. WEAVER  
 DESIGN S. KERN  
 JOB MANAGER D. THOMAS  
 DESIGN SUPERVISOR J. SIMKULET

| APPROACH SLAB TABLE |                                |   |  |
|---------------------|--------------------------------|---|--|
| LOCATION            | CONCRETE (SY)<br>ITEM 557.2009 | LONGITUDINAL SAWCUT<br>GROOVING (SY)<br>ITEM 558.02 | PROTECTIVE<br>SEALER (SF)<br>ITEM 559.01 |
| BEGIN APPROACH SLAB | 19                             | 18  | 169.5                                    |

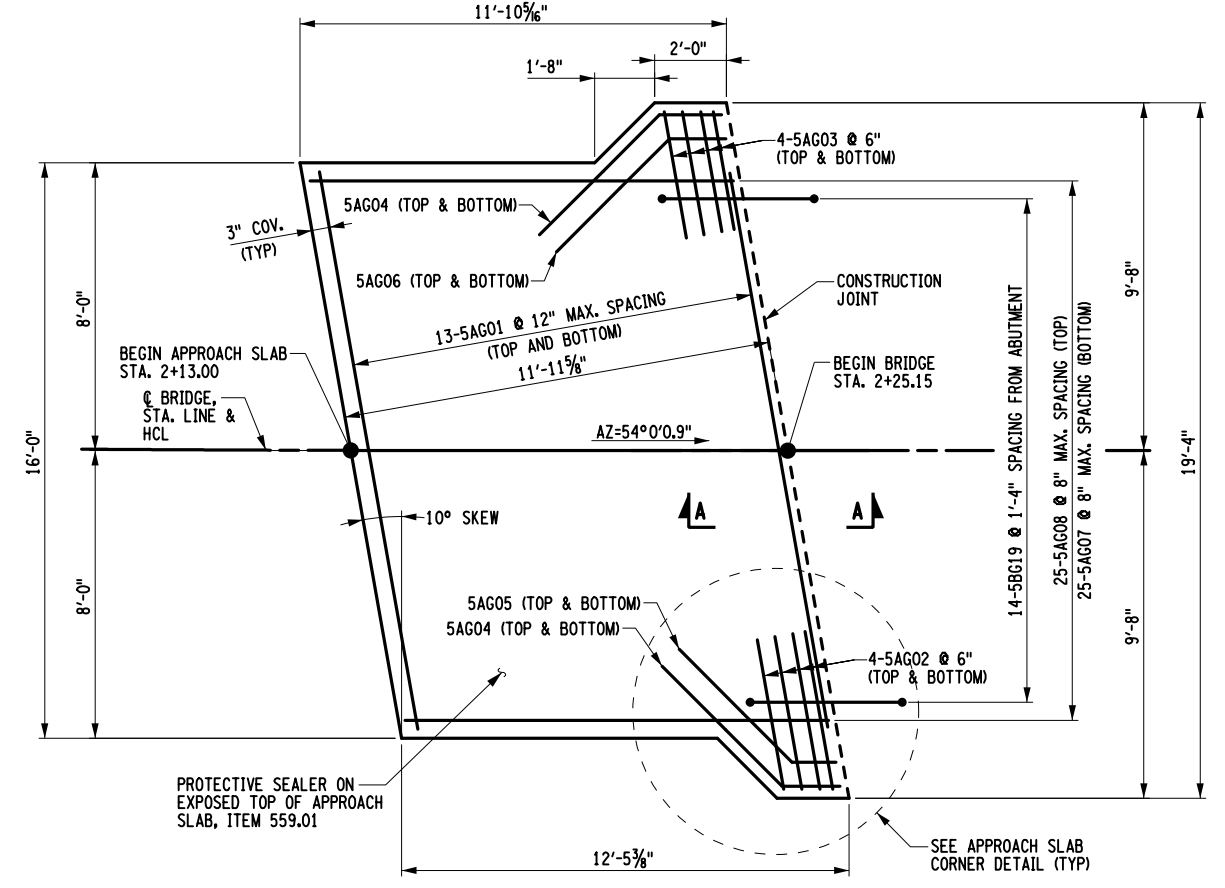


**JOINT RECESS DETAIL**  
NOT TO SCALE

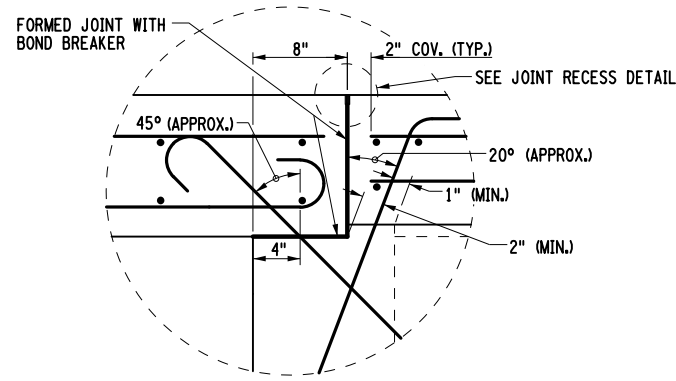
FILL THE RECESS WITH A STRUCTURAL JOINT MATERIAL, SILICONE SEALANT, FROM THE DEPARTMENT'S APPROVED LIST FOR ITEM 567.5100016. IF THE RECESS IS SAW CUT, WATER BLAST IMMEDIATELY FOLLOWING CUTTING TO REMOVE ANY RESIDUAL SLURRY BEFORE IT DRIES. CLEAN THE VERTICAL FACES OF THE RECESS BY ABRASIVE BLAST, AND AIR BLOW THE RESIDUE FROM THE RECESS. PRIME THE VERTICAL FACES WITH THE MANUFACTURER'S RECOMMENDED PRIMER, AND ALLOW TO DRY. PLACE A 3/8" DIA. SOFT CLOSED CELL BACKER ROD IN THE BOTTOM OF THE RECESS. POUR THE SILICONE SEALANT TO A DEPTH OF APPROX. 3/8". PAYMENT TO BE INCLUDED IN THE UNIT PRICE BID FOR THE APPROACH SLAB, ITEM 557.2009.



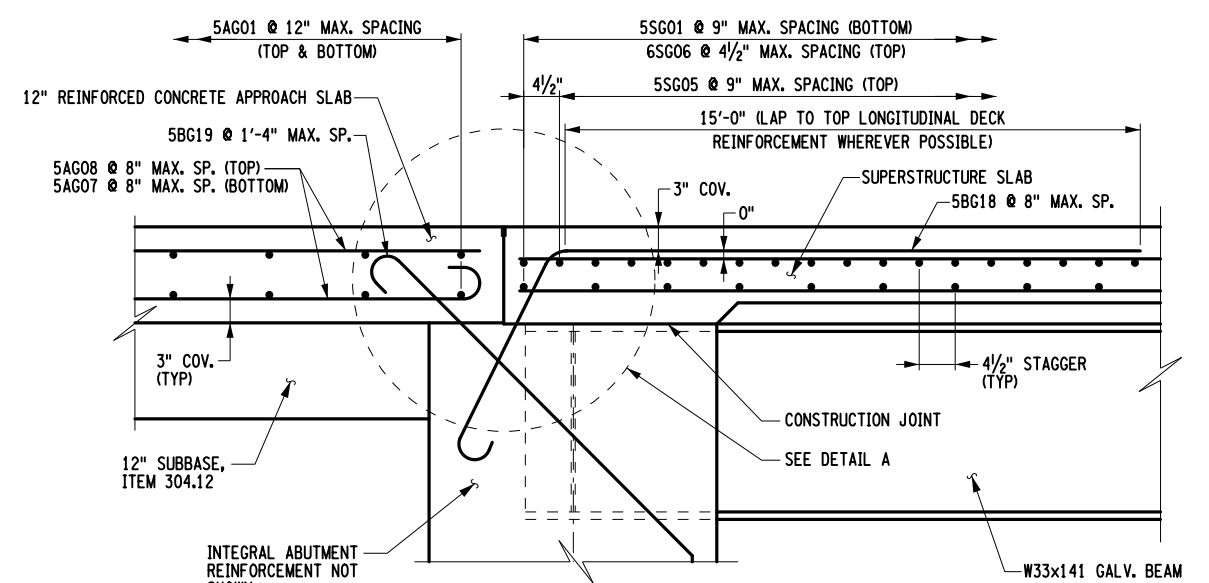
**APPROACH SLAB CORNER DETAIL**  
(MAIN REINFORCEMENT NOT SHOWN)  
SCALE: 1/4"=1'-0"



**BEGIN APPROACH SLAB PLAN**  
SCALE: 3/16"=1'-0"



**DETAIL A**  
NOT TO SCALE



**SECTION A-A**  
SCALE: 1/2"=1'-0"

**NOTES:**

- TO PERMIT UNHINDERED LONGITUDINAL MOVEMENT OF SLAB, THE SURFACE OF THE SUBBASE COURSE MUST BE ACCURATELY CONTROLLED TO FOLLOW AND BE PARALLEL TO THE ROADWAY GRADE AND CROSS SLOPE. POLYETHYLENE CURING COVERS (WHITE OPAQUE) IN ACCORDANCE WITH MATERIAL SPECIFICATION SUBSECTION 711-04 SHALL BE PLACED ON THE FINISHED SUBBASE COURSE THE FULL WIDTH OF THE APPROACH SLAB PRIOR TO PLACEMENT OF THE REINFORCEMENT. THE CURING COVERS SHALL BE 0.004 in. THICK, AND LAPS SHALL BE 2 ft. MINIMUM.
- TOP SURFACES OF STRUCTURAL SLABS AND APPROACH SLABS SHALL BE GROOVED UNDER THE SAWCUT GROOVING OF STRUCTURAL SLAB SURFACE ITEM 558.02

AFFIX SEAL: GREENMAN - PEDERSEN, INC ON: ALTERED BY: ON:



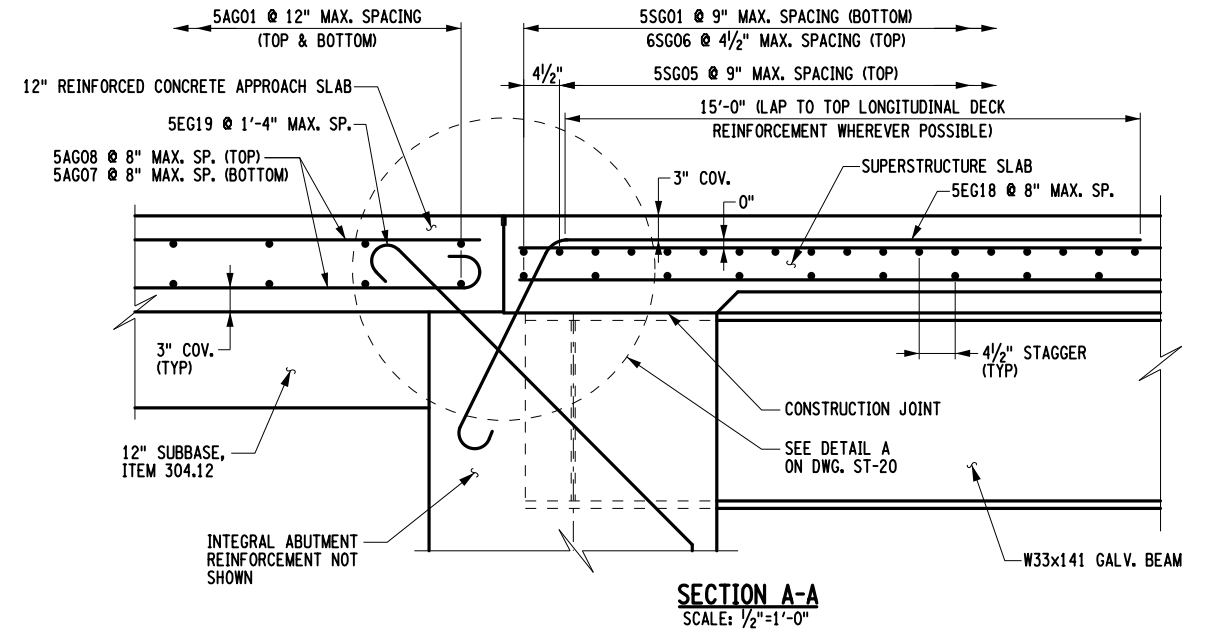
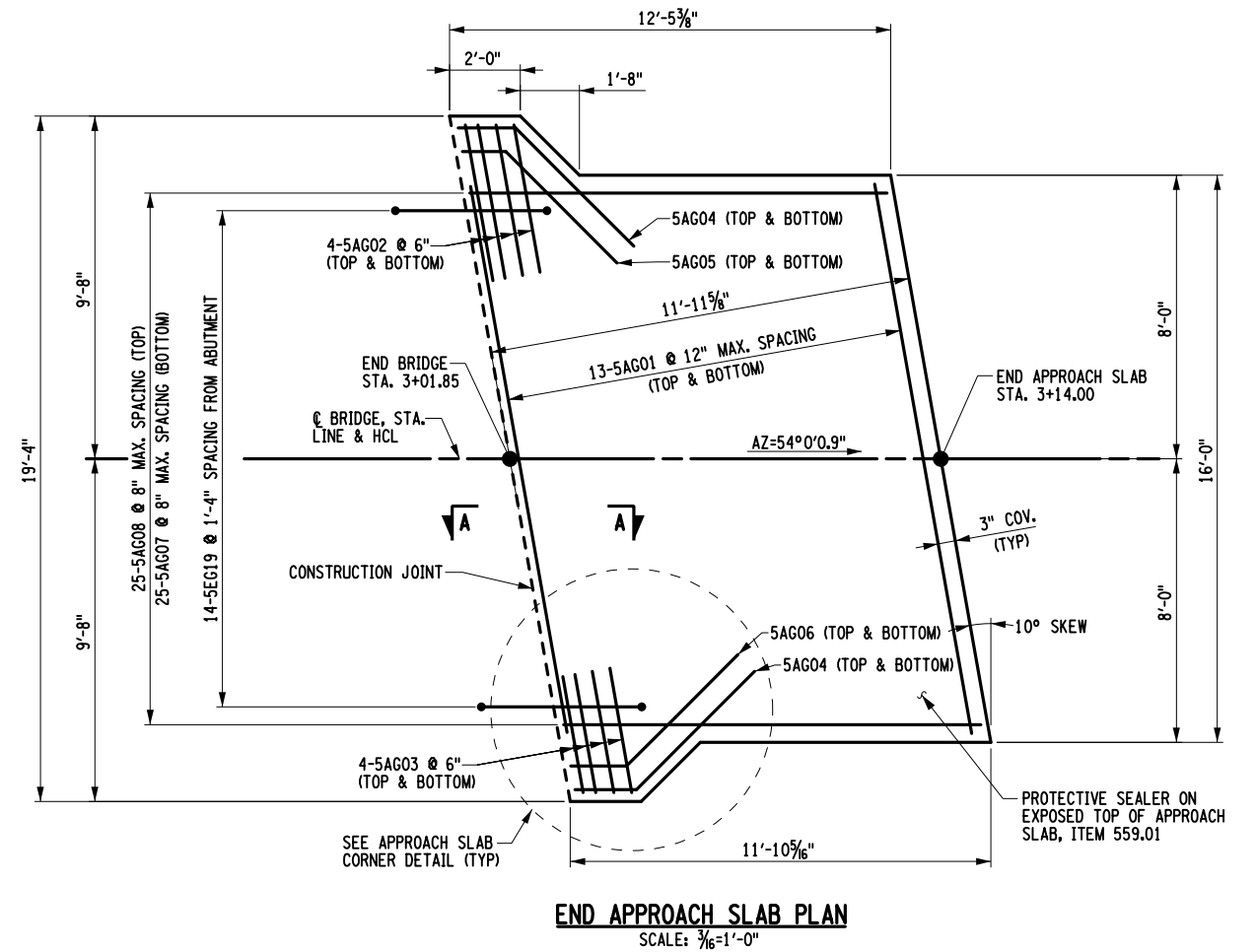
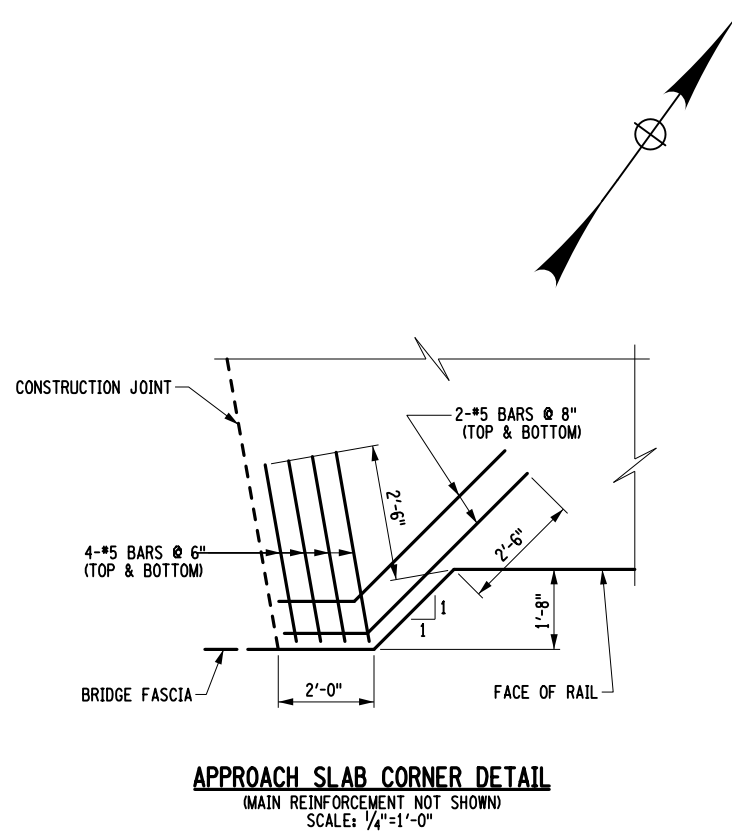
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|--|--|-------------|--------------------|----------|---|--|---|
| AS-BUILT REVISIONS<br>DESCRIPTION OF ALTERATIONS:  | CEMETERY DRIVE OVER THE                    | PIN 2754.54 | BRIDGES<br>3310200 | CULVERTS | ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED | CONTRACT NUMBER<br>D036212                                 |   |
|  | SOUTH CHUCTANUNDA CREEK BRIDGE REPLACEMENT |             |                    |          | BEGIN APPROACH SLAB                         | DRAWING NO. ST-20<br>SHEET NO. 37                          |   |
|  | TOWN OF FLORIDA                            |             |                    |          |   |  |   |
|  | COUNTY: MONTGOMERY                         | REGION: 2   |                    |          |   |  |   |
| IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION. |  |             |                    |          |   | <b>GPI</b> GREENMAN-PEDERSEN, INC.<br>CONSULTING ENGINEERS | MONTGOMERY COUNTY<br>DEPARTMENT OF PUBLIC WORKS |

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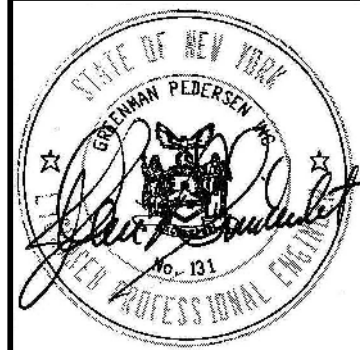
DESIGN SUPERVISOR J. SIMKULET  
 JOB MANAGER D. THOMAS  
 DESIGN S. KERN  
 CHECK B. WEAVER  
 DRAFTING F. CIUCCIO  
 CHECK S. KERN  
 PROJECT MANAGER S. KERN

| APPROACH SLAB TABLE |                                |   |  |
|---------------------|--------------------------------|---|--|
| LOCATION            | CONCRETE (SY)<br>ITEM 557.2009 | LONGITUDINAL SAWCUT<br>GROOVING (SY)<br>ITEM 558.02 | PROTECTIVE<br>SEALER (SF)<br>ITEM 559.01 |
| END APPROACH SLAB   | 19                             | 18  | 169.5                                    |



SEE ST-20 FOR NOTES

AFFIX SEAL: GREENMAN - PEDERSEN, INC ON: ALTERED BY: ON:

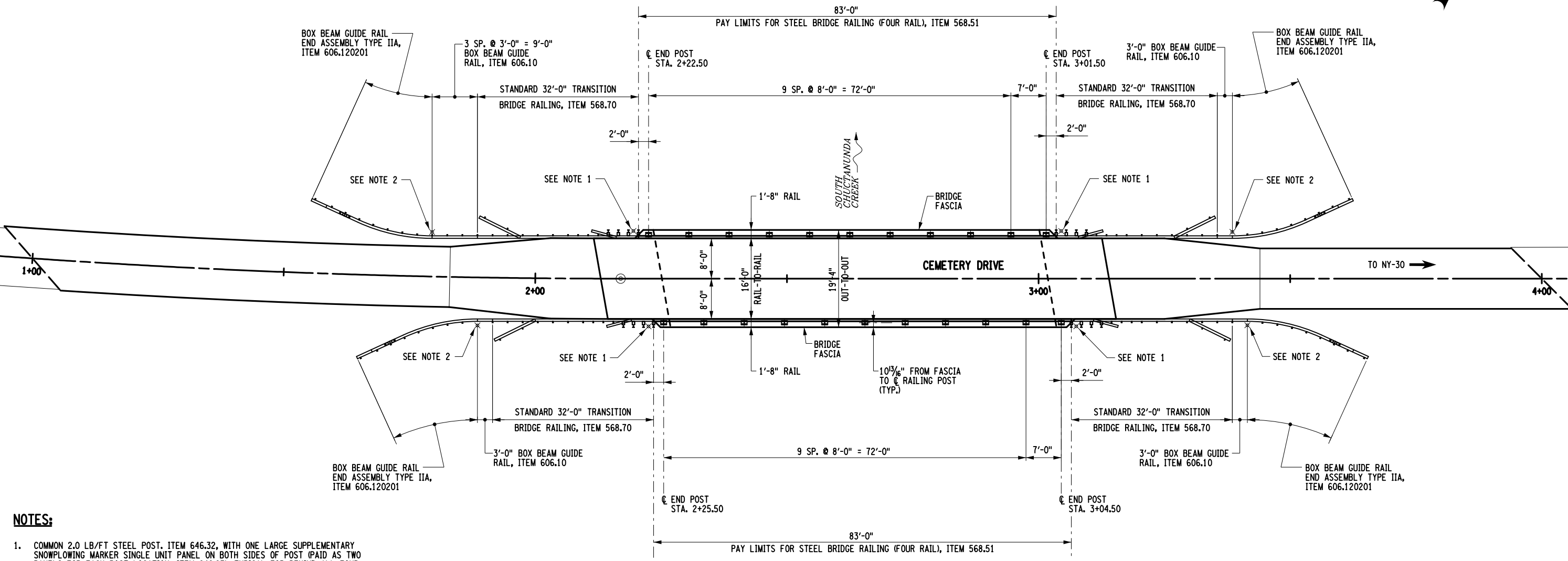
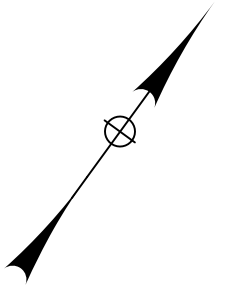


|   |  |             |                 |          |   |                            |
|---|--|-------------|-----------------|----------|---|----------------------------|
| AS-BUILT REVISIONS<br>DESCRIPTION OF ALTERATIONS: | CEMETERY DRIVE OVER THE                    | PIN 2754.54 | BRIDGES 3310200 | CULVERTS | ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED | CONTRACT NUMBER<br>D036212 |
|   | SOUTH CHUCTANUNDA CREEK BRIDGE REPLACEMENT |             |                 |          |   | END APPROACH SLAB          |
|   | TOWN OF FLORIDA                            |             |                 |          |   |                            |
|   | COUNTY: MONTGOMERY REGION: 2               |             |                 |          |   |                            |

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.




PROJECT MANAGER S. KERN  
 CHECK S. KERN  
 DRAFTING F. CIUCCIO  
 CHECK B. WEAVER  
 DESIGN S. KERN  
 JOB MANAGER D. THOMAS  
 DESIGN SUPERVISOR J. SIMKULET





**NOTES:**

1. COMMON 2.0 LB/FT STEEL POST, ITEM 646.32, WITH ONE LARGE SUPPLEMENTARY SNOWPLOWING MARKER SINGLE UNIT PANEL ON BOTH SIDES OF POST (PAID AS TWO PANELS FOR EACH POST LOCATION, ITEM 646.23). TYPICAL FOR BEHIND ALL FOUR TRANSITION RAILINGS BETWEEN FIRST AND SECOND POST AND WHERE SHOWN ON PLANS.
2. COMMON 2.0 LB/FT STEEL POST, ITEM 646.32, WITH ONE LARGE SNOWPLOWING MARKER SINGLE UNIT PANEL ON THE POST SIDE FACING THE BOX BEAM TANGENT SEGMENT, ITEM 646.23, AND ONE LARGE SNOWPLOWING MARKER DOUBLE UNIT PANEL ON THE POST SIDE FACING THE RAILING END ASSEMBLY, ITEM 646.23. TYPICAL AT POINT OF TANGENCY ON ALL RAILING END ASSEMBLIES.

**RAILING PLAN**  
SCALE: 1"=20'

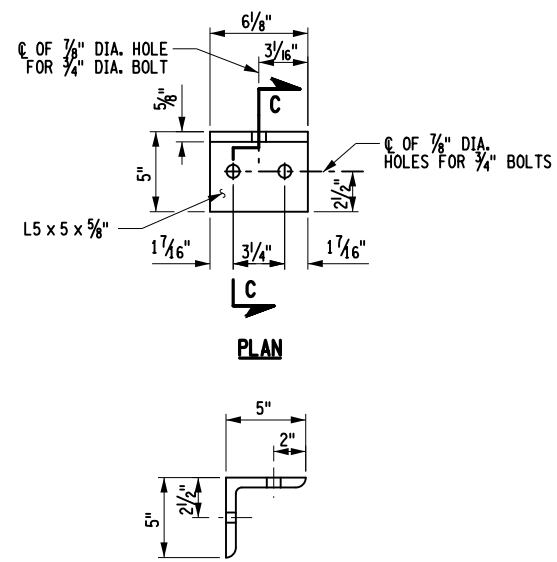
|   |                    |
|---|--------------------|
| AFFIX SEAL: GREENMAN - PEDERSEN, INC<br>ON:   | ALTERED BY:<br>ON: |
|  |                    |

|  |   |             |                    |          |  |   |
|--|---|-------------|--------------------|----------|--|---|
| AS-BUILT REVISIONS<br>DESCRIPTION OF ALTERATIONS:  | CEMETERY DRIVE OVER THE<br>SOUTH CHUCTANUNDA CREEK BRIDGE REPLACEMENT | PIN 2754.54 | BRIDGES<br>3310200 | CULVERTS | ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED  | CONTRACT NUMBER<br>D036212  |
|  | TOWN OF FLORIDA<br>COUNTY: MONTGOMERY REGION: 2                       |             |                    |          | RAILING PLAN   | DRAWING NO. ST-22<br>SHEET NO. 39   |
| IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION. |   |             |                    |          |  <b>GREENMAN-PEDERSEN, INC.</b><br>CONSULTING ENGINEERS |  MONTGOMERY COUNTY<br>DEPARTMENT OF PUBLIC WORKS |

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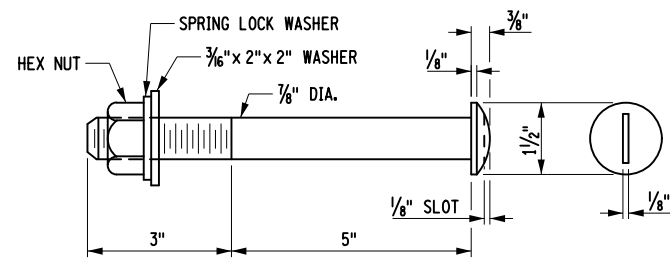
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 USER = skern

DESIGN SUPERVISOR J. SIMKULET  
 JOB MANAGER D. THOMAS  
 DESIGN S. KERN  
 CHECK B. WEAVER  
 DRAFTING F. CUOCCIO  
 CHECK S. KERN  
 PROJECT MANAGER S. KERN



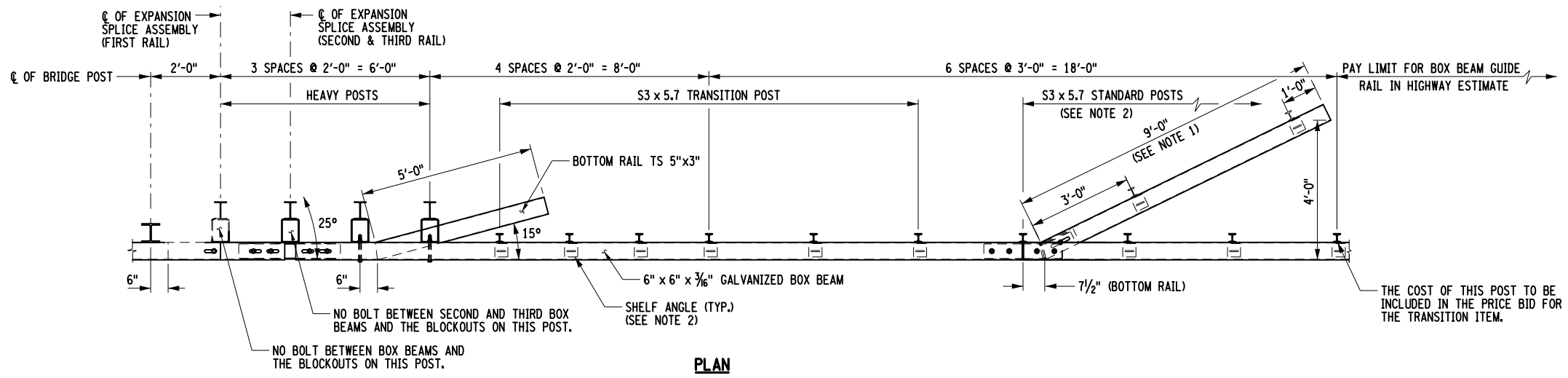
SECTION C-C

RAILING ANGLE DETAILS  
 NOT TO SCALE

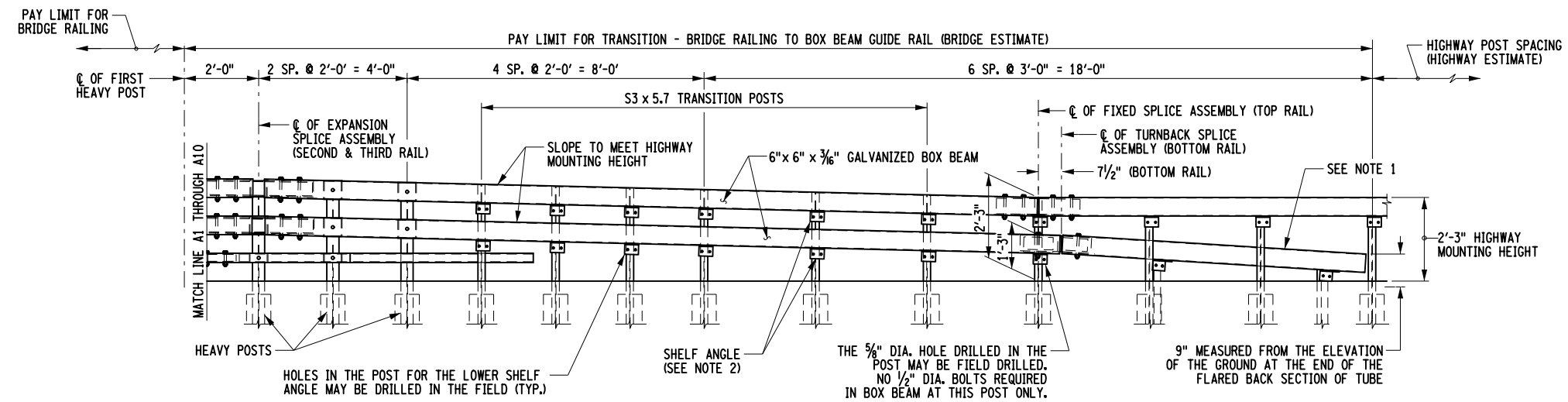


SLOTTED ROUND HEAD BOLT  
 NOT TO SCALE

- NOTES:**
1. THE COST OF THE POSTS, SPLICE TUBE AND RAIL FOR THE LOWER TUBE FLARE SECTION IS INCLUDED IN THE PRICE BID FOR THE TRANSITION ITEM.
  2. SEE TYPICAL RAIL TO POST CONNECTION DETAIL ON CURRENT HIGHWAY STANDARD SHEET TITLED "BOX BEAM GUIDE RAIL".

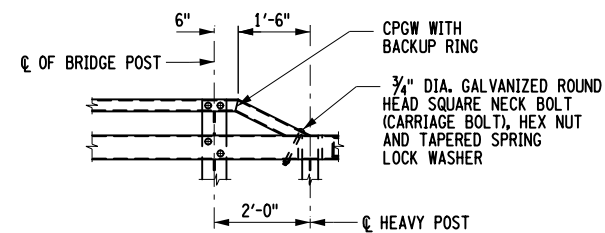


PLAN

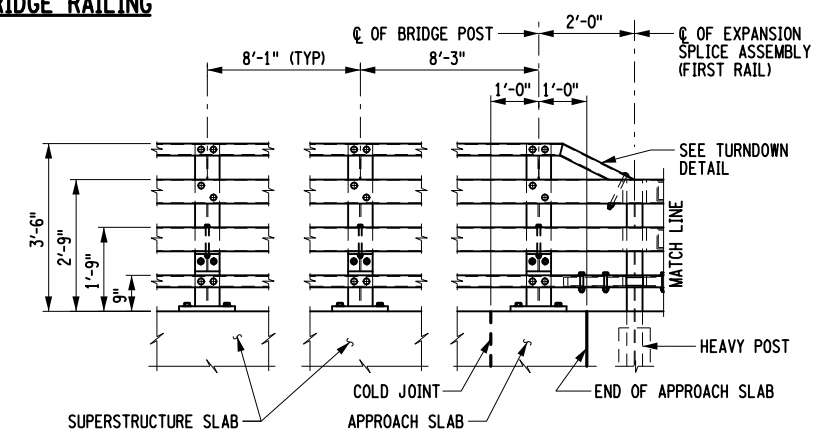


ELEVATION

STANDARD TRANSITION BRIDGE RAILING  
 NOT TO SCALE

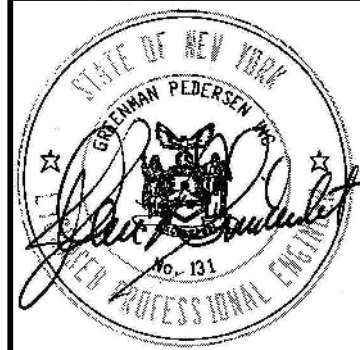


TURNDOWN DETAIL  
 NOT TO SCALE



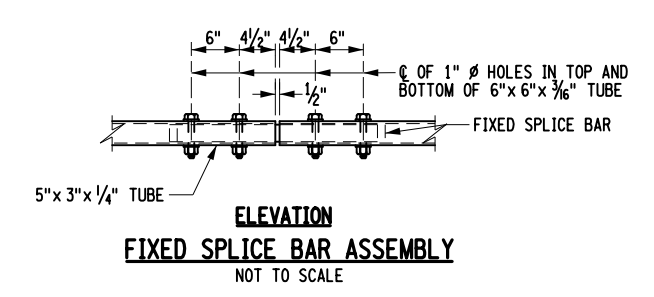
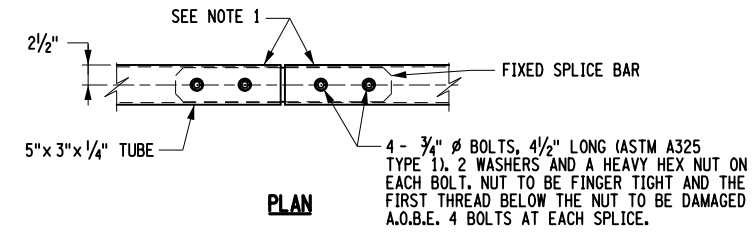
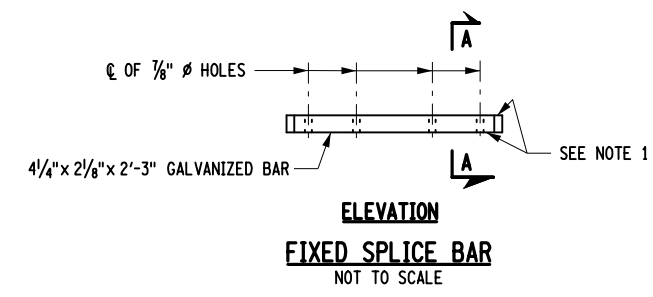
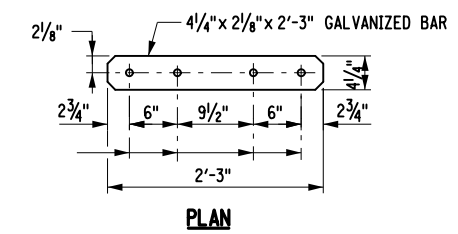
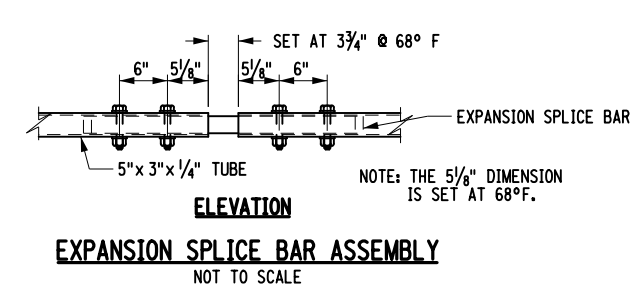
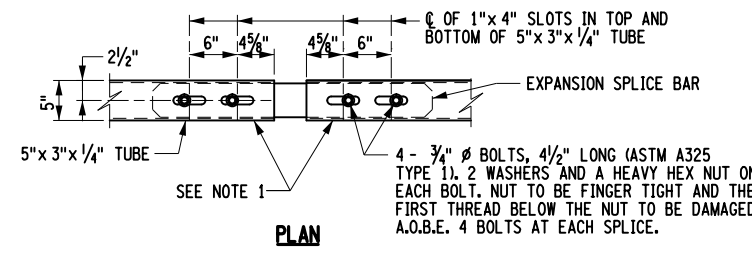
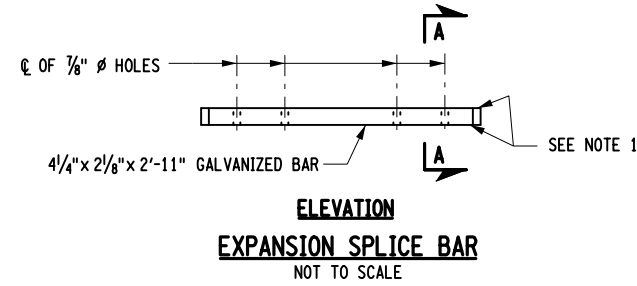
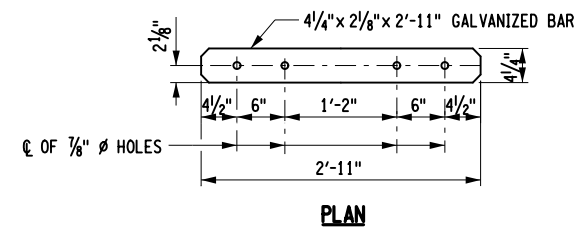
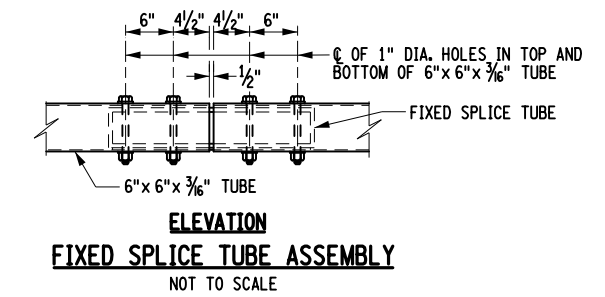
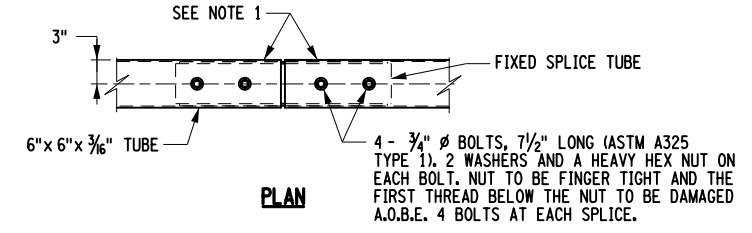
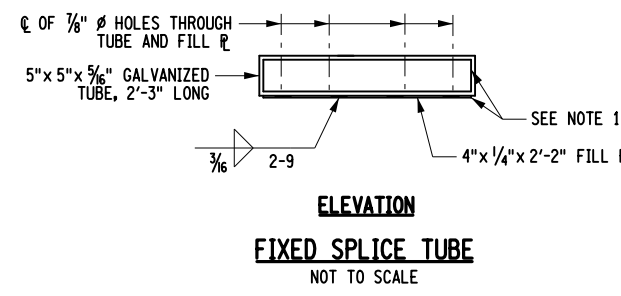
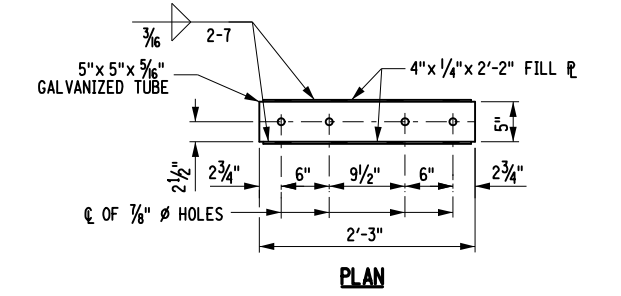
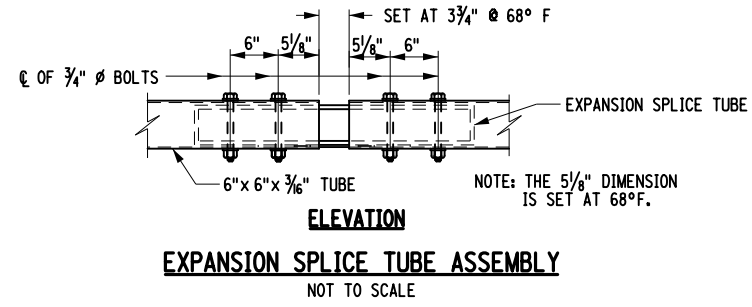
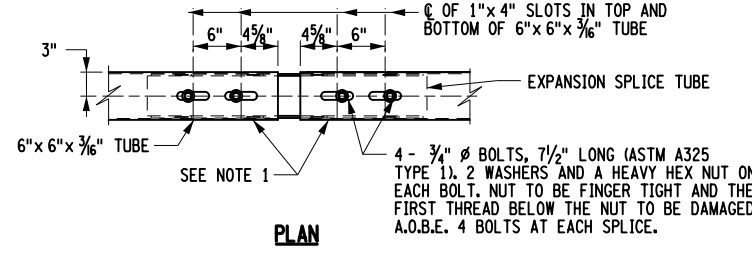
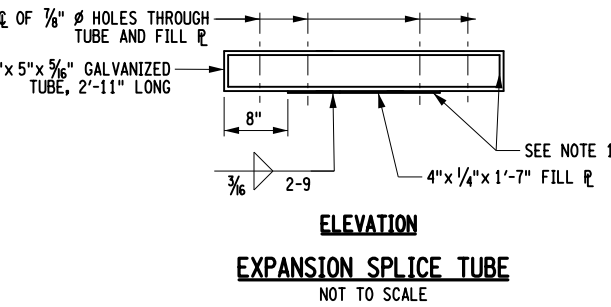
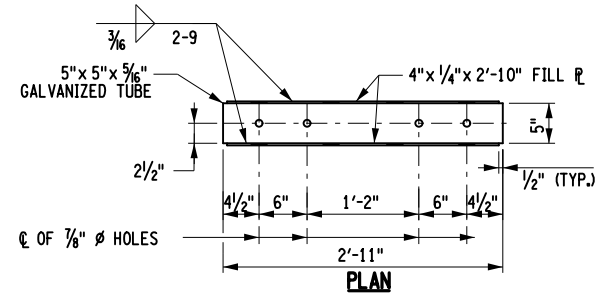
ELEVATION MATCH FOUR-RAIL - CURBLESS  
 NOT TO SCALE

AFFIX SEAL: GREENMAN - PEDERSEN, INC  
 ON: ALTERED BY: ON:



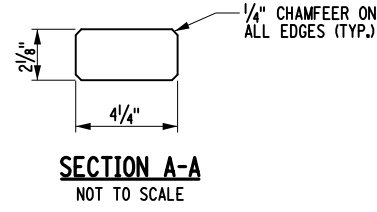
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| AS-BUILT REVISIONS<br>DESCRIPTION OF ALTERATIONS:  | CEMETERY DRIVE OVER THE                    | PIN 2754.54 | BRIDGES<br>3310200 | CULVERTS | ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED | CONTRACT NUMBER<br>D036212                          |   |
|  | SOUTH CHUCTANUNDA CREEK BRIDGE REPLACEMENT |             |                    |          | RAILING DETAILS<br>(1 OF 3)                 | DRAWING NO. ST-23<br>SHEET NO. 40                   |   |
|  | TOWN OF FLORIDA                            |             |                    |          |   |   |   |
|  | COUNTY: MONTGOMERY                         | REGION: 2   |                    |          |   |   |   |
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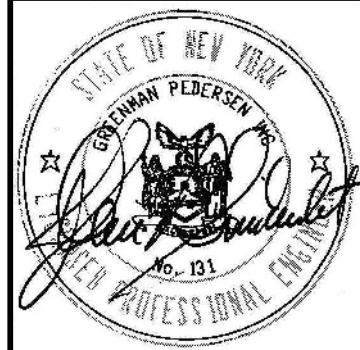


**NOTES:**

1. PROTRUSIONS CAUSED BY WELDING OR GALVANIZING ARE NOT PERMITTED ON THE ADJOINING SURFACES OF THE BOX BEAM RAILS, SPLICE TUBES AND FILL PLATES.



AFFIX SEAL: GREENMAN - PEDERSEN, INC ON: ALTERED BY: ON:

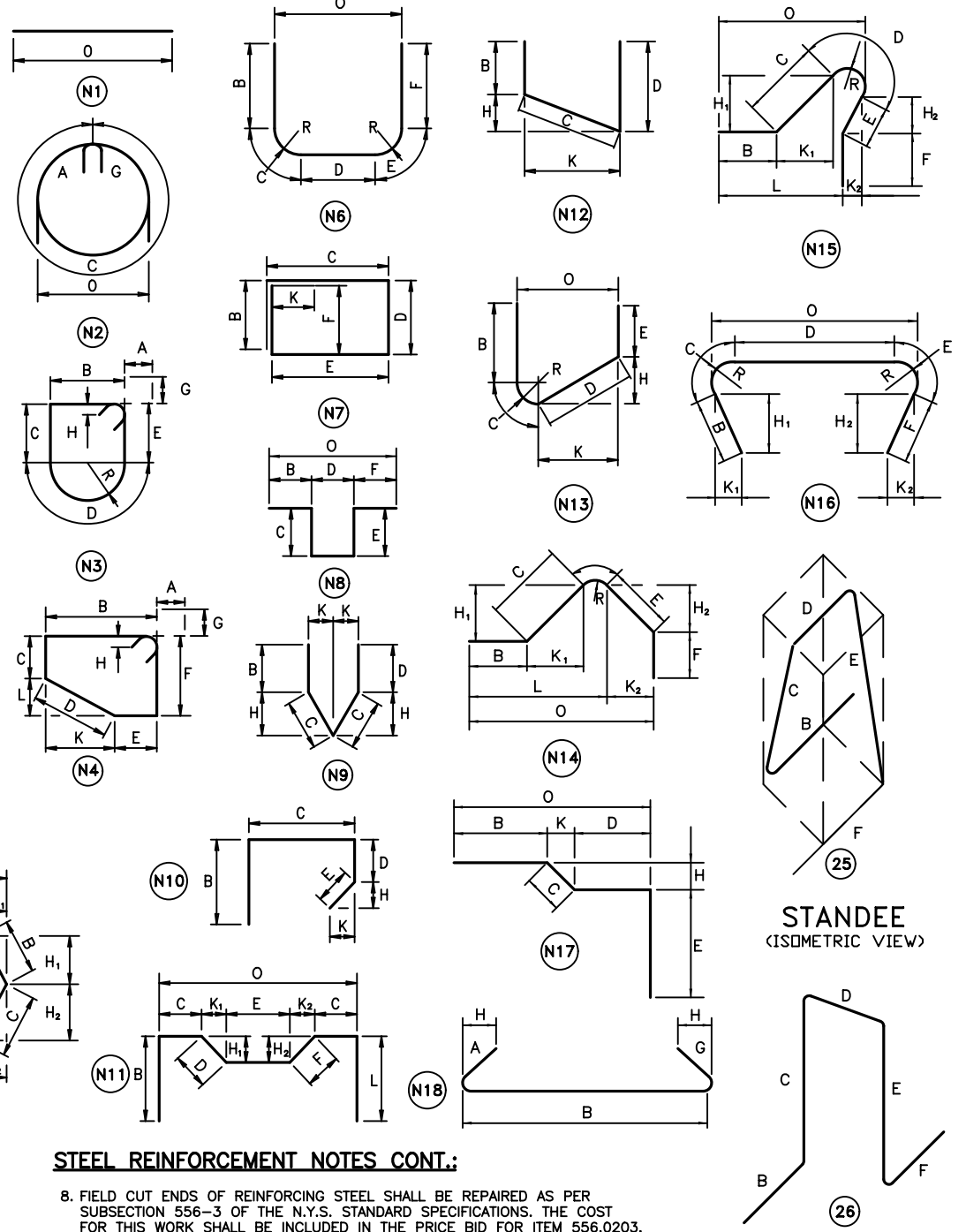
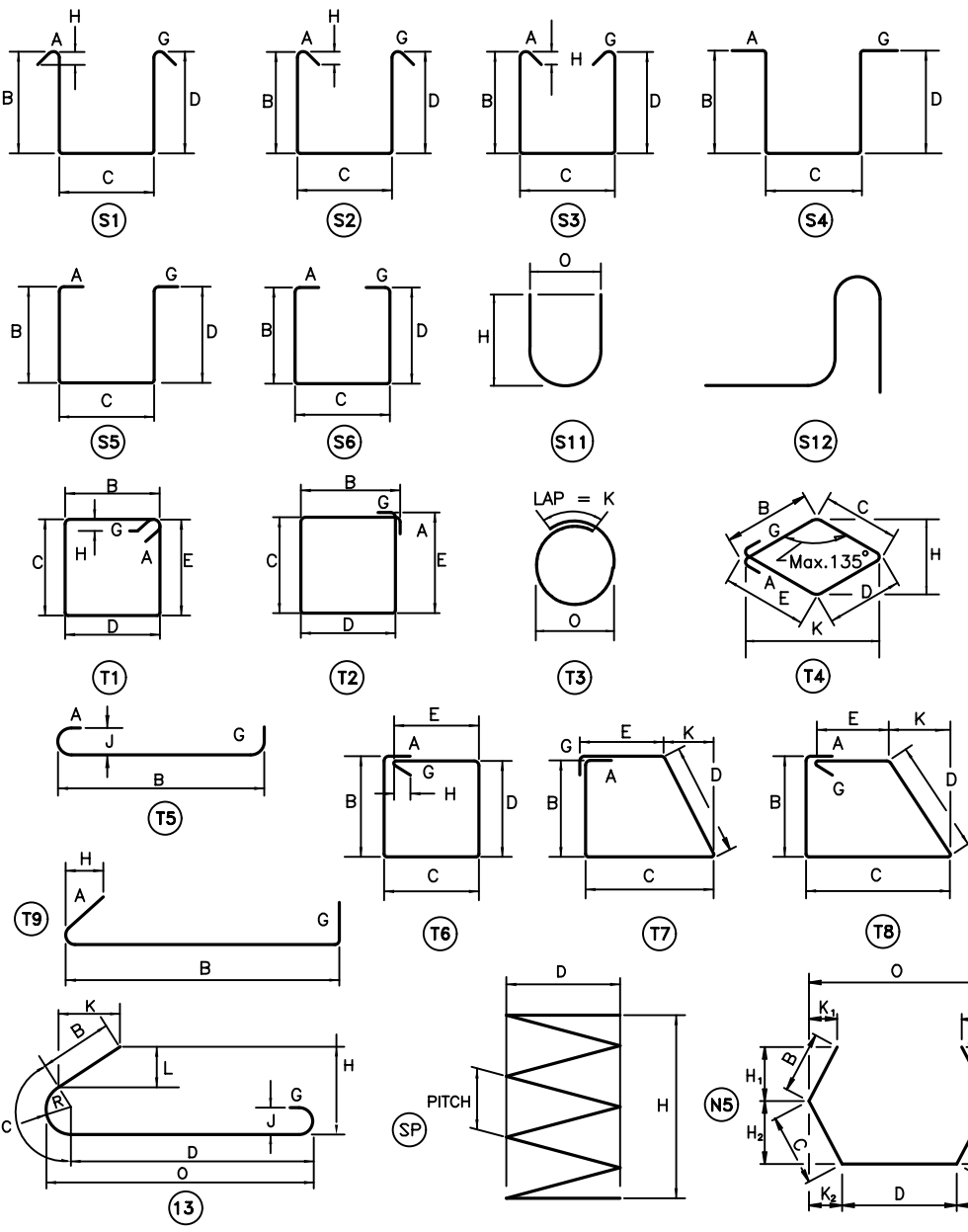
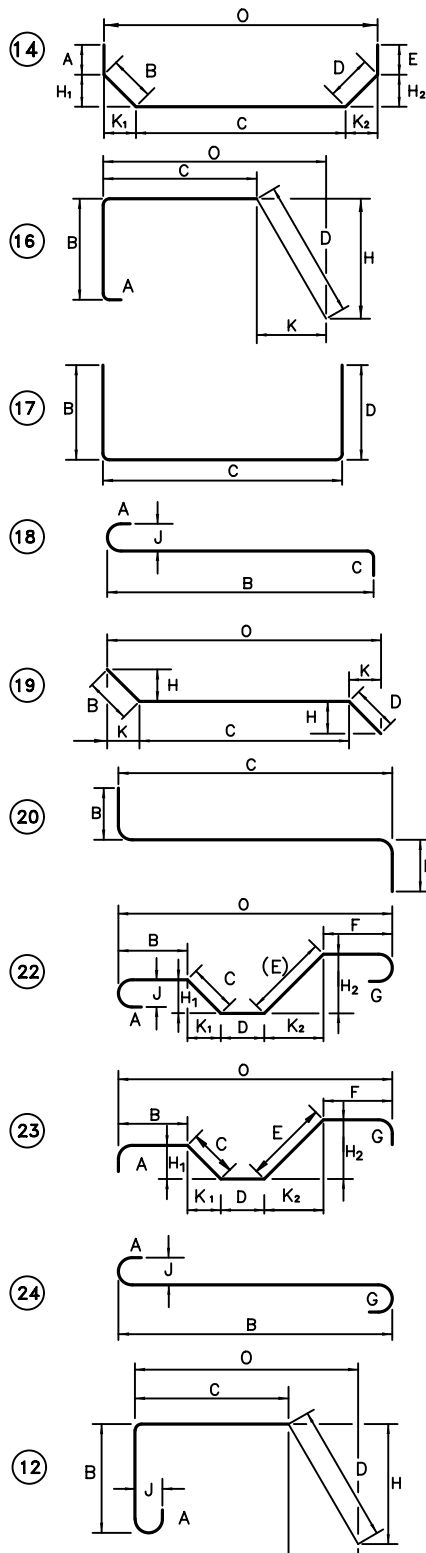
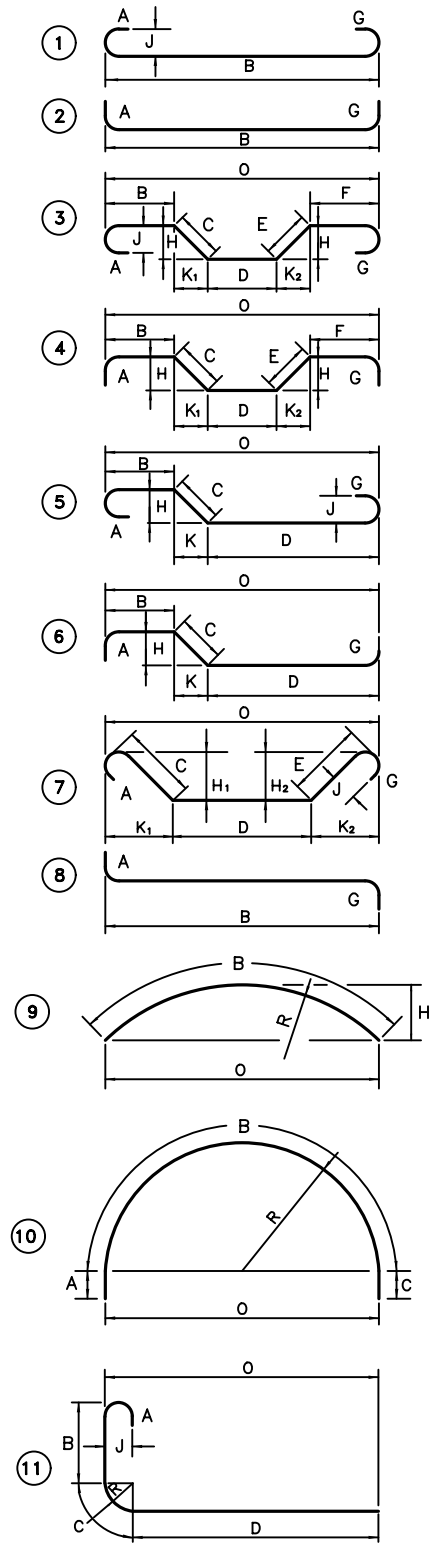


|                             |  |             |                 |          |   |                         |
|-----------------------------|--|-------------|-----------------|----------|---|-------------------------|
| AS-BUILT REVISIONS          | CEMETERY DRIVE OVER THE                    | PIN 2754.54 | BRIDGES 3310200 | CULVERTS | ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED | CONTRACT NUMBER D036212 |
| DESCRIPTION OF ALTERATIONS: | SOUTH CHUCTANUNDA CREEK BRIDGE REPLACEMENT |             |                 |          | RAILING DETAILS (3 OF 3)                    | DRAWING NO. ST-25       |
|                             | TOWN OF FLORIDA                            |             |                 |          |   | SHEET NO. 42            |
|                             | COUNTY: MONTGOMERY                         | REGION: 2   |                 |          |   |                         |

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

**GPI** GREENMAN-PEDERSEN, INC. CONSULTING ENGINEERS

MONTGOMERY COUNTY DEPARTMENT OF PUBLIC WORKS



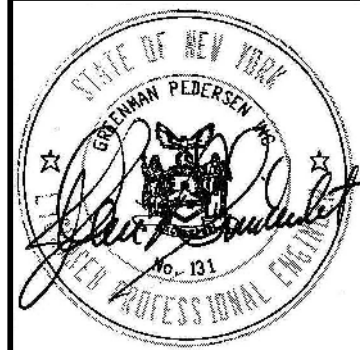
**STEEL REINFORCEMENT NOTES:**

- UNLESS OTHERWISE DESIGNATED, ALL BAR REINFORCEMENT FOR CONCRETE IN SIZES UP TO AND INCLUDING NO. 18 SHALL CONFORM TO THE REQUIREMENTS OF THE "SPECIFICATIONS FOR DEFORMED BILLET-STEEL BARS FOR CONCRETE REINFORCEMENT", ASTM A615-06a. ALL BARS SHALL BE GRADE 60, UNLESS OTHERWISE DESIGNATED.
- FOR TYPICAL BENDING DETAILS, RECOMMENDED P.I.N. DIAMETER "D" OF BENDS HOOKS AND OTHER STANDARD PRACTICES SEE ACI-318-05 & THE CONCRETE REINFORCING STEEL INSTITUTE (CRSI) "MANUAL OF STANDARD PRACTICES" (MSP).
- BARS WHICH REQUIRE MORE ACCURATE BENDING THAN STANDARD PRACTICES SHOULD HAVE LIMITS INDICATED ON THE BARLIST.
- ALL DIMENSIONS ARE OUT TO OUT OF BAR EXCEPT "A" AND "G" ON STANDARD 180° AND 135° HOOKS.
- DIMENSIONS "A","G" AND "J" ARE STANDARD BENDING DIMENSIONS PER EACH SIZE OF BAR. REFER TO C.R.S.I. - M.S.P. FOR DETAILS.
- WHERE SLOPE DIFFERS FROM 45°, DIMENSIONS "H" AND "K" MUST BE SHOWN ON BARLIST.
- ▲ - INDICATES BARS TO CUT TO FIT IN THE FIELD.

**STEEL REINFORCEMENT NOTES CONT.:**

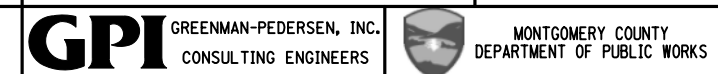
- FIELD CUT ENDS OF REINFORCING STEEL SHALL BE REPAIRED AS PER SUBSECTION 556-3 OF THE N.Y.S. STANDARD SPECIFICATIONS. THE COST FOR THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR ITEM 556.0203.
- ITEM FOR REINFORCING STEEL IS AS FOLLOWS:  
556.0203 - GALVANIZED BAR REINFORCEMENT FOR STRUCTURES.
- BAR MARK PROCEDURE IS AS FOLLOWS:  
5BG03  
 — SEQUENTIAL BAR NUMBER.  
 — INDICATES BAR COATING (G=GALVANIZED)  
 — INDICATES STRUCTURE TYPE AND/OR LOCATION (B=BEGIN ABT, E=END ABT, S=SUPERSTRUCTURE SLAB, A=APPROACH SLAB)  
 — INDICATES BAR SIZE (IE. 5,6,7,ETC.)
- BAR MARKS SHOULD NORMALLY INCREASE IN SEQUENTIAL NUMBER AND SIZE FROM SMALLEST TO LARGEST AT EACH LOCATION.
- "\*" - INDICATES BAR MARK IS OUT OF SEQUENCE.

AFFIX SEAL: GREENMAN - PEDERSEN, INC ON: \_\_\_\_\_  
 ALTERED BY: ON: \_\_\_\_\_



|   |   |             |                    |          |   |                            |
|---|---|-------------|--------------------|----------|---|----------------------------|
| AS-BUILT REVISIONS<br>DESCRIPTION OF ALTERATIONS: | CEMETERY DRIVE OVER THE<br>SOUTH CHUCTANUNDA CREEK BRIDGE REPLACEMENT | PIN 2754.54 | BRIDGES<br>3310200 | CULVERTS | ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED | CONTRACT NUMBER<br>D036212 |
|   | TOWN OF FLORIDA   |             |                    |          | BAR BENDS                                   | DRAWING NO. ST-26          |
|   | COUNTY: MONTGOMERY  | REGION: 2   |                    |          |   | SHEET NO. 43               |

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.




| MARK                           | NO. | SIZE | LENGTH | TYPE | WEIGHT  | A    | B     | C | D | E | F | G  | H/H1 | H2 | J  | K/K1 | K2 | L | O | R | REMARKS |
|--------------------------------|-----|------|--------|------|---------|------|-------|---|---|---|---|----|------|----|----|------|----|---|---|---|---------|
| SUPERSTRUCTURE SLAB            |     |      |        |      |         |      |       |   |   |   |   |    |      |    |    |      |    |   |   |   |         |
| SSG01                          | 102 | 5    | 19'-3" | STR  | 2,047.9 |      |       |   |   |   |   |    |      |    |    |      |    |   |   |   |         |
| SSG02                          | 39  | 5    | 40'-0" | STR  | 1,627.1 |      |       |   |   |   |   |    |      |    |    |      |    |   |   |   |         |
| SSG03                          | 39  | 5    | 25'-0" | STR  | 1,016.9 |      |       |   |   |   |   |    |      |    |    |      |    |   |   |   |         |
| SSG04                          | 39  | 5    | 16'-0" | STR  | 650.8   |      |       |   |   |   |   |    |      |    |    |      |    |   |   |   |         |
| SSG05                          | 101 | 5    | 17'-3" | STR  | 1,817.2 |      |       |   |   |   |   |    |      |    |    |      |    |   |   |   |         |
| 6SG06                          | 406 | 6    | 5'-11" | 1    | 3,608.1 | 8"   | 5'-3" |   |   |   |   | 0" |      |    | 6" |      |    |   |   |   |         |
| TOTAL FOR SUPERSTRUCTURE SLAB: |     |      |        |      | 10,768  | LBS. |       |   |   |   |   |    |      |    |    |      |    |   |   |   |         |



| MARK                           | NO. | SIZE | LENGTH | TYPE | WEIGHT | A    | B      | C     | D     | E | F | G  | H/H1    | H2 | J  | K/K1    | K2 | L | O | R        | REMARKS                                    |
|--------------------------------|-----|------|--------|------|--------|------|--------|-------|-------|---|---|----|---------|----|----|---------|----|---|---|----------|--|
| BEGIN APPROACH SLAB            |     |      |        |      |        |      |        |       |       |   |   |    |         |    |    |         |    |   |   |          |  |
| SAG01                          | 26  | 5    | 15'-8" | STR  | 424.8  |      |        |       |       |   |   |    |         |    |    |         |    |   |   |          |  |
| SAG02                          | 8   | 5    | VARIES | STR  | 36.3   |      |        |       |       |   |   |    |         |    |    |         |    |   |   |          | MAX. LENGTH = 4'-6"; MIN. LENGTH = 4'-2.5" |
| SAG03                          | 8   | 5    | VARIES | STR  | 28.7   |      |        |       |       |   |   |    |         |    |    |         |    |   |   |          | MAX. LENGTH = 3'-7"; MIN. LENGTH = 3'-3.5" |
| SAG04                          | 4   | 5    | 6'-4"  | 16   | 26.4   | 0"   | 0"     | 1'-7" | 4'-9" |   |   |    | 3'-4.3" |    |    | 3'-4.3" |    |   |   | 4'-11.3" |  |
| SAG05                          | 2   | 5    | 5'-8"  | 16   | 11.8   | 0"   | 0"     | 1'-2" | 4'-6" |   |   |    | 3'-2.2" |    |    | 3'-2.2" |    |   |   | 4'-4.2"  |  |
| SAG06                          | 2   | 5    | 6'-0"  | 16   | 12.5   | 0"   | 0"     | 1'-6" | 4'-6" |   |   |    | 3'-2.2" |    |    | 3'-2.2" |    |   |   | 4'-8.2"  |  |
| SAG07                          | 25  | 5    | 12'-2" | 1    | 317.2  | 7"   | 11'-7" |       |       |   |   | 0" |         |    | 5" |         |    |   |   |          |  |
| SAG08                          | 25  | 5    | 11'-7" | STR  | 302.0  |      |        |       |       |   |   |    |         |    |    |         |    |   |   |          |  |
| TOTAL FOR BEGIN APPROACH SLAB: |     |      |        |      | 1,160  | LBS. |        |       |       |   |   |    |         |    |    |         |    |   |   |          |  |

| MARK                         | NO. | SIZE | LENGTH | TYPE | WEIGHT | A    | B      | C     | D     | E | F | G  | H/H1    | H2 | J  | K/K1    | K2 | L | O | R        | REMARKS                                    |
|------------------------------|-----|------|--------|------|--------|------|--------|-------|-------|---|---|----|---------|----|----|---------|----|---|---|----------|--|
| END APPROACH SLAB            |     |      |        |      |        |      |        |       |       |   |   |    |         |    |    |         |    |   |   |          |  |
| SAG01                        | 26  | 5    | 15'-8" | STR  | 424.8  |      |        |       |       |   |   |    |         |    |    |         |    |   |   |          |  |
| SAG02                        | 8   | 5    | VARIES | STR  | 36.3   |      |        |       |       |   |   |    |         |    |    |         |    |   |   |          | MAX. LENGTH = 4'-6"; MIN. LENGTH = 4'-2.5" |
| SAG03                        | 8   | 5    | VARIES | STR  | 28.7   |      |        |       |       |   |   |    |         |    |    |         |    |   |   |          | MAX. LENGTH = 3'-7"; MIN. LENGTH = 3'-3.5" |
| SAG04                        | 4   | 5    | 6'-4"  | 16   | 26.4   | 0"   | 0"     | 1'-7" | 4'-9" |   |   |    | 3'-4.3" |    |    | 3'-4.3" |    |   |   | 4'-11.3" |  |
| SAG05                        | 2   | 5    | 5'-8"  | 16   | 11.8   | 0"   | 0"     | 1'-2" | 4'-6" |   |   |    | 3'-2.2" |    |    | 3'-2.2" |    |   |   | 4'-4.2"  |  |
| SAG06                        | 2   | 5    | 6'-0"  | 16   | 12.5   | 0"   | 0"     | 1'-6" | 4'-6" |   |   |    | 3'-2.2" |    |    | 3'-2.2" |    |   |   | 4'-8.2"  |  |
| SAG07                        | 25  | 5    | 12'-2" | 1    | 317.2  | 7"   | 11'-7" |       |       |   |   | 0" |         |    | 5" |         |    |   |   |          |  |
| SAG08                        | 25  | 5    | 11'-7" | STR  | 302.0  |      |        |       |       |   |   |    |         |    |    |         |    |   |   |          |  |
| TOTAL FOR END APPROACH SLAB: |     |      |        |      | 1,160  | LBS. |        |       |       |   |   |    |         |    |    |         |    |   |   |          |  |

**NOTES:**

- SLAB REINFORCEMENT IS TO BE GALVANIZED AND PAID UNDER THE APPROPRIATE SLAB ITEM.

|   |                    |
|---|--------------------|
| AFFIX SEAL: GREENMAN - PEDERSEN, INC<br>ON:   | ALTERED BY:<br>ON: |
|  |                    |


|   |   |             |                    |          |   |   |
|---|---|-------------|--------------------|----------|---|---|
| AS-BUILT REVISIONS<br>DESCRIPTION OF ALTERATIONS: | CEMETERY DRIVE OVER THE<br>SOUTH CHUCTANUNDA CREEK BRIDGE REPLACEMENT | PIN 2754.54 | BRIDGES<br>3310200 | CULVERTS | ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED   | CONTRACT NUMBER<br>D036212  |
|   | TOWN OF FLORIDA   |             |                    |          | BAR LIST<br>(1 OF 2)  | DRAWING NO. ST-27<br>SHEET NO. 44   |
|   | COUNTY: MONTGOMERY  | REGION: 2   |                    |          |  GREENMAN-PEDERSEN, INC.<br>CONSULTING ENGINEERS |  MONTGOMERY COUNTY<br>DEPARTMENT OF PUBLIC WORKS |

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.



**NOTES:**

1. ABUTMENT REINFORCEMENT IS TO BE GALVANIZED AND PAID UNDER ITEM 556.0203.

|   |                    |
|---|--------------------|
| AFFIX SEAL: GREENMAN - PEDERSEN, INC<br>ON:   | ALTERED BY:<br>ON: |
|  |                    |

| MARK                                    | NO. | SIZE | LENGTH | TYPE | WEIGHT  | A    | B      | C     | D      | E | F | G | H/H1 | H2 | J | K/K1 | K2 | L | O | R | REMARKS |
|---|-----|------|--------|------|---------|------|--------|-------|--------|---|---|---|------|----|---|------|----|---|---|---|---------|
| BEGIN ABUTMENT LOWER POUR - PLACEMENT 1 |     |      |        |      |         |      |        |       |        |   |   |   |      |    |   |      |    |   |   |   |         |
| 8BG01                                   | 57  | 8    | 17'-0" | 17   | 2,587.2 |      | 7'-2"  | 2'-8" | 7'-2"  |   |   |   |      |    |   |      |    |   |   |   |         |
| 5BG02                                   | 2   | 5    | 28'-0" | STR  | 58.4    |      |        |       |        |   |   |   |      |    |   |      |    |   |   |   |         |
| 5BG03                                   | 2   | 5    | 16'-9" | STR  | 34.9    |      |        |       |        |   |   |   |      |    |   |      |    |   |   |   |         |
| 6BG04                                   | 16  | 6    | 28'-0" | STR  | 672.9   |      |        |       |        |   |   |   |      |    |   |      |    |   |   |   |         |
| 6BG05                                   | 16  | 6    | 17'-3" | STR  | 414.6   |      |        |       |        |   |   |   |      |    |   |      |    |   |   |   |         |
| 8BG06                                   | 16  | 8    | 28'-0" | STR  | 1,196.2 |      |        |       |        |   |   |   |      |    |   |      |    |   |   |   |         |
| 8BG07                                   | 16  | 8    | 18'-6" | STR  | 790.3   |      |        |       |        |   |   |   |      |    |   |      |    |   |   |   |         |
| 5BG08                                   | 43  | 5    | 9'-4"  | 17   | 418.6   |      | 3'-4"  | 2'-8" | 3'-4"  |   |   |   |      |    |   |      |    |   |   |   |         |
| 8BG09                                   | 33  | 8    | 15'-0" | 17   | 1,321.7 |      | 6'-2"  | 2'-8" | 6'-2"  |   |   |   |      |    |   |      |    |   |   |   |         |
| 5BG10                                   | 48  | 5    | VARIES | STR  | 269.1   |      |        |       |        |   |   |   |      |    |   |      |    |   |   |   |         |
| 6BG11                                   | 24  | 6    | 10'-4" | 17   | 372.5   |      | 3'-10" | 2'-8" | 3'-10" |   |   |   |      |    |   |      |    |   |   |   |         |
| TOTAL FOR PLACEMENT 1:                  |     |      |        |      | 8,137   | LBS. |        |       |        |   |   |   |      |    |   |      |    |   |   |   |         |

| MARK                                  | NO. | SIZE | LENGTH | TYPE | WEIGHT  | A    | B      | C     | D      | E | F | G | H/H1 | H2 | J | K/K1 | K2 | L | O | R | REMARKS |
|---------------------------------------|-----|------|--------|------|---------|------|--------|-------|--------|---|---|---|------|----|---|------|----|---|---|---|---------|
| END ABUTMENT LOWER POUR - PLACEMENT 2 |     |      |        |      |         |      |        |       |        |   |   |   |      |    |   |      |    |   |   |   |         |
| 8EG01                                 | 57  | 8    | 17'-0" | 17   | 2,587.2 |      | 7'-2"  | 2'-8" | 7'-2"  |   |   |   |      |    |   |      |    |   |   |   |         |
| 5EG02                                 | 2   | 5    | 28'-0" | STR  | 58.4    |      |        |       |        |   |   |   |      |    |   |      |    |   |   |   |         |
| 5EG03                                 | 2   | 5    | 16'-9" | STR  | 34.9    |      |        |       |        |   |   |   |      |    |   |      |    |   |   |   |         |
| 6EG04                                 | 16  | 6    | 28'-0" | STR  | 672.9   |      |        |       |        |   |   |   |      |    |   |      |    |   |   |   |         |
| 6EG05                                 | 16  | 6    | 17'-3" | STR  | 414.6   |      |        |       |        |   |   |   |      |    |   |      |    |   |   |   |         |
| 8EG06                                 | 16  | 8    | 28'-0" | STR  | 1,196.2 |      |        |       |        |   |   |   |      |    |   |      |    |   |   |   |         |
| 8EG07                                 | 16  | 8    | 18'-6" | STR  | 790.3   |      |        |       |        |   |   |   |      |    |   |      |    |   |   |   |         |
| 5EG08                                 | 43  | 5    | 9'-4"  | 17   | 418.6   |      | 3'-4"  | 2'-8" | 3'-4"  |   |   |   |      |    |   |      |    |   |   |   |         |
| 8EG09                                 | 33  | 8    | 14'-6" | 17   | 1,277.6 |      | 5'-11" | 2'-8" | 5'-11" |   |   |   |      |    |   |      |    |   |   |   |         |
| 5EG10                                 | 48  | 5    | VARIES | STR  | 262.8   |      |        |       |        |   |   |   |      |    |   |      |    |   |   |   |         |
| 6EG11                                 | 24  | 6    | 10'-4" | 17   | 372.5   |      | 3'-10" | 2'-8" | 3'-10" |   |   |   |      |    |   |      |    |   |   |   |         |
| TOTAL FOR PLACEMENT 2:                |     |      |        |      | 8,087   | LBS. |        |       |        |   |   |   |      |    |   |      |    |   |   |   |         |

| MARK                                    | NO. | SIZE | LENGTH | TYPE | WEIGHT | A    | B      | C      | D      | E | F | G  | H/H1   | H2 | J  | K/K1   | K2 | L | O      | R | REMARKS |
|---|-----|------|--------|------|--------|------|--------|--------|--------|---|---|----|--------|----|----|--------|----|---|--------|---|---------|
| BEGIN ABUTMENT UPPER POUR - PLACEMENT 3 |     |      |        |      |        |      |        |        |        |   |   |    |        |    |    |        |    |   |        |   |         |
| 6BG12                                   | 3   | 6    | 28'-0" | STR  | 126.2  |      |        |        |        |   |   |    |        |    |    |        |    |   |        |   |         |
| 6BG13                                   | 3   | 6    | 16'-5" | STR  | 74.0   |      |        |        |        |   |   |    |        |    |    |        |    |   |        |   |         |
| 8BG14                                   | 8   | 8    | 28'-0" | STR  | 598.1  |      |        |        |        |   |   |    |        |    |    |        |    |   |        |   |         |
| 8BG15                                   | 8   | 8    | 17'-5" | STR  | 372.0  |      |        |        |        |   |   |    |        |    |    |        |    |   |        |   |         |
| 6BG16                                   | 6   | 6    | 5'-0"  | STR  | 45.1   |      |        |        |        |   |   |    |        |    |    |        |    |   |        |   |         |
| 6BG17                                   | 4   | 6    | 12'-0" | STR  | 72.1   |      |        |        |        |   |   |    |        |    |    |        |    |   |        |   |         |
| 5BG18                                   | 30  | 5    | 18'-3" | 5    | 571.0  | 7"   | 2'-8"  | 15'-0" | 0"     |   |   | 0" | 14'-1" |    | 0" | 5'-2"  |    |   | 7'-10" |   |         |
| 5BG19                                   | 14  | 5    | 5'-10" | 5    | 85.2   | 7"   | 4'-3"  | 1'-0"  | 0"     |   |   | 0" | 10"    |    | 0" | 7"     |    |   | 4'-10" |   |         |
| 6BG20                                   | 2   | 6    | 10'-4" | 17   | 31.0   |      | 3'-10" | 2'-8"  | 3'-10" |   |   |    |        |    |    |        |    |   |        |   |         |
| 6BG21                                   | 4   | 6    | 3'-9"  | STR  | 22.5   |      |        |        |        |   |   |    |        |    |    |        |    |   |        |   |         |
| 6BG22                                   | 8   | 6    | 13'-9" | 16   | 165.2  | 0"   | 2'-9"  | 1'-7"  | 9'-5"  |   |   |    | 3'-3"  |    |    | 8'-10" |    |   | 10'-5" |   |         |
| 5BG23                                   | 24  | 5    | VARIES | 17   | 158.5  |      | VARIES | 2'-8"  | VARIES |   |   |    |        |    |    |        |    |   |        |   |         |
| TOTAL FOR PLACEMENT 3:                  |     |      |        |      | 2,321  | LBS. |        |        |        |   |   |    |        |    |    |        |    |   |        |   |         |

| MARK                                  | NO. | SIZE | LENGTH | TYPE | WEIGHT | A    | B      | C      | D      | E | F | G  | H/H1   | H2 | J  | K/K1   | K2 | L | O      | R | REMARKS |
|---------------------------------------|-----|------|--------|------|--------|------|--------|--------|--------|---|---|----|--------|----|----|--------|----|---|--------|---|---------|
| END ABUTMENT UPPER POUR - PLACEMENT 4 |     |      |        |      |        |      |        |        |        |   |   |    |        |    |    |        |    |   |        |   |         |
| 6EG12                                 | 3   | 6    | 28'-0" | STR  | 126.2  |      |        |        |        |   |   |    |        |    |    |        |    |   |        |   |         |
| 6EG13                                 | 3   | 6    | 16'-5" | STR  | 74.0   |      |        |        |        |   |   |    |        |    |    |        |    |   |        |   |         |
| 8EG14                                 | 8   | 8    | 28'-0" | STR  | 598.1  |      |        |        |        |   |   |    |        |    |    |        |    |   |        |   |         |
| 8EG15                                 | 8   | 8    | 17'-5" | STR  | 372.0  |      |        |        |        |   |   |    |        |    |    |        |    |   |        |   |         |
| 6EG16                                 | 6   | 6    | 5'-0"  | STR  | 45.1   |      |        |        |        |   |   |    |        |    |    |        |    |   |        |   |         |
| 6EG17                                 | 4   | 6    | 12'-0" | STR  | 72.1   |      |        |        |        |   |   |    |        |    |    |        |    |   |        |   |         |
| 5EG18                                 | 30  | 5    | 18'-3" | 5    | 571.0  | 7"   | 2'-8"  | 15'-0" | 0"     |   |   | 0" | 14'-1" |    | 0" | 5'-2"  |    |   | 7'-10" |   |         |
| 5EG19                                 | 14  | 5    | 5'-10" | 5    | 85.2   | 7"   | 4'-3"  | 1'-0"  | 0"     |   |   | 0" | 10"    |    | 0" | 7"     |    |   | 4'-10" |   |         |
| 6EG20                                 | 2   | 6    | 10'-4" | 17   | 31.0   |      | 3'-10" | 2'-8"  | 3'-10" |   |   |    |        |    |    |        |    |   |        |   |         |
| 6EG21                                 | 4   | 6    | 3'-0"  | STR  | 18.0   |      |        |        |        |   |   |    |        |    |    |        |    |   |        |   |         |
| 6EG22                                 | 8   | 6    | 13'-8" | 16   | 164.2  | 0"   | 2'-9"  | 1'-7"  | 9'-4"  |   |   |    | 3'-0"  |    |    | 8'-10" |    |   | 10'-5" |   |         |
| 5EG23                                 | 24  | 5    | VARIES | 17   | 158.5  |      | VARIES | 2'-8"  | VARIES |   |   |    |        |    |    |        |    |   |        |   |         |
| TOTAL FOR PLACEMENT 4:                |     |      |        |      | 2,316  | LBS. |        |        |        |   |   |    |        |    |    |        |    |   |        |   |         |

|  |  |             |                    |          |  |   |
|--|--|-------------|--------------------|----------|--|---|
| AS-BUILT REVISIONS<br>DESCRIPTION OF ALTERATIONS:  | CEMETERY DRIVE OVER THE                    | PIN 2754.54 | BRIDGES<br>3310200 | CULVERTS | ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED                | CONTRACT NUMBER<br>D036212                      |
|  | SOUTH CHUCTANUNDA CREEK BRIDGE REPLACEMENT |             |                    |          |  |   |
|  | TOWN OF FLORIDA                            |             |                    |          |  |   |
|  | COUNTY: MONTGOMERY                         | REGION: 2   |                    |          | BAR LIST<br>(2 OF 2)                                       | DRAWING NO. ST-28<br>SHEET NO. 45               |
| IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION. |  |             |                    |          | <b>GPI</b> GREENMAN-PEDERSEN, INC.<br>CONSULTING ENGINEERS | MONTGOMERY COUNTY<br>DEPARTMENT OF PUBLIC WORKS |